



Village of Hazel Crest

Transit Oriented Development Plan

October 2002





Hazel Crest Transit Oriented Development Plan

Prepared By:



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Disclaimer

This document summarizes work conducted for the Village of Hazel Crest Transit Oriented Development Plan. This document was prepared by Teska Associates, Inc. under contract to the Regional Transportation Authority. Preparation of this document was financed in part through a grant from the U. S. Department of Transportation, Federal Transit Administration, under the Federal Transit Act and/or the Illinois Department of Transportation, and the Regional Transportation Authority. The contents do not necessarily reflect the official views of the U. S. Department of Transportation, Federal Transit Administration, the Illinois Department of Transportation, or the Regional Transportation Authority.



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Introduction

Purpose

The goal of this project is to develop a concept plan, design criteria and strategies for redevelopment within a ½ mile radius of the Hazel Crest Metra Station. This area is locally known as Hazel Crest Proper. The concept plan addresses such issues as circulation, parking and pedestrian access, recommended physical improvements, redevelopment opportunities, and commuter-parking opportunities within the study area. The Regional Transportation Authority (RTA) under the Regional Technical Assistance Program funded the project.

This study is a follow-up to two previous planning studies, the Hazel Crest Proper Revitalization Action Plan completed in November 2000 by Business Districts, Inc. and Teska Associates, Inc. and the Hazel Crest Proper Redevelopment Area Plan and Program that was completed in December 2000 by Teska Associates, Inc. This last plan resulted in the creation of a Tax Increment Finance (TIF) District for key blocks within the larger station area.

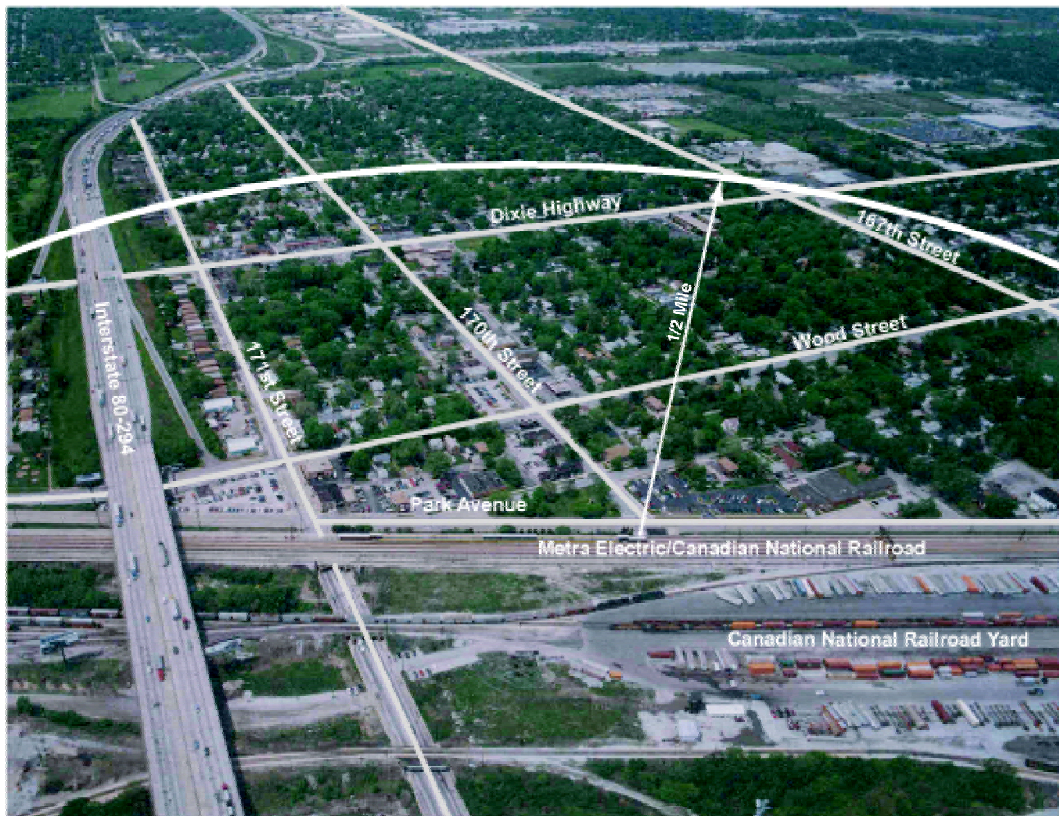


Figure 1: Oblique Air Photo



Introduction

Specific project goals and objectives include the following:

Goal: Enhance the physical environment in the station area.

Objectives:

- Encourage private property owners to make building and site improvements that meet a higher standard of design and maintenance.
- Improve public property by way of improvements to streets and utilities.
- Prepare and implement streetscape plans and other public improvements.

Goal: Strengthen the Hazel Crest Proper Redevelopment Area as an element of the social and economic base of the community.

Objectives:

- Redevelop key opportunity sites.
- Consider appropriate opportunities for Village financial involvement in redevelopment.
- Pursue business and developer recruitment for existing and redeveloped properties.
- Control crime through active police and neighborhood watch programs.
- Explore potential for development of senior housing and a community center.
- Cooperate with the Park District in the creation of a play ground in or near the center of the neighborhood.
- Provide additional parking for Metra riders and customers of study area businesses.

Goal: Encourage the location of goods and services within the Hazel Crest Proper Redevelopment Area to meet the needs of the community.

Objectives:

- Work with the Village's Economic Development Task Force to implement plan recommendations.
- Fund façade improvement and recruitment programs.



Introduction

- Build on excellent multi-modal transportation access to attract successful businesses.

Approach

This study consists of three basic elements:

A **Situation Audit** that summarizes existing conditions and trends within the study area. Key elements of this effort included implementation of a Metra ridership survey, review and updating of past market evaluations and physical conditions in the study area, and review of successful redevelopment efforts in other communities.

A **Redevelopment Strategy** that outlines public and private sector physical improvements in the study area, along with appropriate redevelopment strategies for key parcels within Hazel Crest Proper.

An **Implementation Strategy** that details phasing, funding responsibilities, and other key issues that must be addressed to carry the plan to fruition.

The plan was developed by a consulting team lead by Teska Associates, Inc. Other consulting team members included the marketing and management firm of Business Districts, Inc. and the Fish Transportation Group. The consultant team worked closely with a project team comprised of representatives from the Village of Hazel Crest, Metra and the RTA.

A key element of this project was to ensure broad-based community input and support so that plans and recommendations from the study will be pragmatic and become the foundation for future implementation efforts by the Village. The Village has already completed considerable outreach to the neighborhood in prior planning studies. As a part of this study, neighborhood residents and business owners were invited to participate in a hands-on planning charette in January 2002, designed to generate potential redevelopment ideas. The workshop grouped participants into teams. Each team was asked to develop a plan for the study area using a set of 3-dimensional blocks representing alternative building types and sizes. Other opportunities for community input included an open house to review the draft plan and a presentation to the Hazel Crest Village Board.

In addition to the plans and sketches contained in this document, a computer-based urban simulation has been created. This simulation was prepared, as an independent study, by Crystal Wilson, a Masters of Urban Planning student at the University of Illinois at Chicago, as a part of her thesis. The simulation consists of a 3D model of the built environment within a half-mile radius of the Hazel Crest Metra station. It allows viewing of existing conditions, and evaluation of alternative development proposals within the context of the existing neighborhood.



Situation Audit

Trade Area Summary

The market for businesses in Hazel Crest Proper, its retail trade area, is based on a number of factors, including the mix and draw of existing stores; quality, quantity and location of competitive shopping areas; ease of access; physical and perceptual barriers. The trade area in Hazel Crest Proper is also influenced by the location of the Metra Station. Retail businesses located near transit or Metra stations have expanded trade areas. A larger marketshed is created by park-n-ride commuters who travel from different communities. In addition, the business area is enhanced due to visibility by train passengers.

Hazel Crest Proper's Primary Trade Area

Since Hazel Crest Proper is a traditional neighborhood, it has the potential to capture more activity from the neighborhood than a typical suburban subdivision. There are sidewalks, front porches, variety in housing styles and prices, and nearby mixed-use commercial development. All of these elements are classic components of the traditional neighborhood layout many builders try to capture in today's most trendy developments. Residents can walk to shopping, restaurants and public transportation. However, Hazel Crest Proper differs from the trendy new developments in that it fell out of fashion during the last 20 years as Hazel Crest developed newer housing and consumers began shopping in malls. The strategy recommended as part of this study will suggest ways to capitalize on the benefits of being a pedestrian-oriented traditional neighborhood.

Key demographic features of the station area include:

Population	3,183
Daytime Population	2,122
Median Household Income	\$45,728
Average Household Income	\$43,134
Median Age	32.6
Annual Restaurant Sales (Mil)	\$6.3
Annual Grocery/Drug Sales (Mil)	\$10.3
Annual Home Furnishing Sales (Mil)	\$1.6

Source: Experian/Applied Geographic Solutions



Situation Audit

By itself, this small residential market only provides enough spending power to support a few quick-serve restaurants and convenience businesses. In contrast to the relatively small residential population, the daytime employee market in Hazel Crest Proper is larger than would be expected in communities with three to four times the residential population. These employees are important because they offer area businesses the opportunity to attract customers during daytime hours when many suburban businesses experience lagging sales. The more periods of high traffic, the higher sales potential for a business.

The strategy section of this report will identify ways to take full advantage of the current populations and use development to build additional customer populations.

Hazel Crest Proper's Total Trade Area

The total trade area population is composed of individuals who will travel to Hazel Crest Proper for specialty items not available in their primary shopping location. After they arrive, these shoppers may also frequent convenience businesses. With the development of nearby, competitive town centers like Homewood, Park Forest, and Lansing, as well as physical barriers like Interstate 294, today's trade area is much smaller, a 15-20 minute drive.

Since many residents in this area are strongly drawn to regional shopping centers like Lincoln Mall or River Oaks Center, a reasonable goal for Hazel Crest Proper is attracting five percent of the total trade area spending. This goal is based on results in well developed and attractive downtowns such as Elmhurst, Evanston and Oak Park. The population characteristics and spending potentials of the residents within a three mile ring from the center of Hazel Crest Proper include:

Population	98,140
Daytime Population	40,525
Median Household Income	\$52,964
Average Household Income	\$57,964
Median Age	33.9
Annual Restaurant Sales (Mil)	\$208.7
Annual Grocery/Drug Sales (Mil)	\$344.8
Annual Home Furnishing Sales (Mil)	\$52.7

Source: Experian/Applied Geographic Solutions

Metra commuters provide another addition to the total market for Hazel Crest Proper; approximately 700 people board the train at the Hazel Crest Station each day. Those who board at the station are thought by Metra to provide 5 to 10% of the sales made by



Situation Audit

station area businesses¹. The nearly 9,000² passengers on the trains passing the station are exposed to the businesses in the station area everyday. If that exposure creates a positive impression, then those customers will return to visit the businesses noticed through the train windows.

The population available to shop in Hazel Crest Proper matches the requirements of many national retailers. This plan recommends techniques to improve the available space and the area's image to Metra riders and others using the highly traveled roads in the study area.

Existing Land Use

The primary land use within the study area is residential. However, within the broad land use category of residential there is a diversity of housing types. Two significant multifamily residential developments occupy the I-80/294 frontage. Several small apartment buildings are also interspersed with the commercial development along 170th Street. The Purhman Trailer Park, is located at the corner of 169th Street and Dixie Highway. The primarily single-family residential neighborhoods consist of moderately dense single-family homes on small lots (5,000 SF).

Commercial development along Dixie Highway is dominated by automobile oriented businesses including a hotdog restaurant and a strip commercial center. An office building is also part of this corridor. The businesses concentrated along northern Park Avenue include several small commercial structures, an existing Metra building, and an outdoor storage facility. These buildings have good visibility from the elevated rail line. 170th Street supports mainly small businesses such as hair and nail salons, insurance companies, a convenience store and one bar as well as a gas station and restaurant. Several of these buildings have some interesting architectural character. However, while most buildings are structurally sound, some facade rehabilitation is needed to enhance or restore the character of several of these businesses.

There are several major public uses within this area including two schools, two major municipal uses and several churches. The Warren Palm Elementary School is located at the northeast corner of Dixie Highway and 170th Street. St. Anne's School is located one block north. Hazel Crest Fire Station Number 1 is located at the northwest corner of Dixie Highway and 170th Street and Water Pumping Station Number 1 is located at the southwest corner of 167th Street and Park Avenue. A major new church has completed construction in a plot of vacant land on 167th Street east of Wood Street. A landscape company is under construction on Park Avenue.



The southern portion of Park Avenue is home to several outdated commercial buildings.

¹"Local Impact in Commuter Rail Station Areas," Metra, December 1994

²"Commuter Rail System Station Boarding/Alighting Count," Metra, January 1999



Situation Audit

Strength and Challenge Analysis

Strengths

The elements of Hazel Crest that enhance its attractiveness as a location for Transit Oriented Development include:

- Traditional Neighborhood
A traditional neighborhood design increases pedestrian movement.
- Development Sites North along the Metra Tracks
As the Hazel Crest Proper Tax Increment Financing (TIF) Redevelopment Plan illustrates, there are opportunities to enhance Hazel Crest by redevelopment of underutilized sites. These sites can provide additional customers through creation of more retail, new office, or multifamily residential.
- Feeder Bus Service
Pace provides one feeder bus route to the Hazel Crest Metra Station affording a greater number of individuals access to area businesses and Metra service. Bus routes are illustrated on the Planning Issues Map (Figure 3).
- Hazel Crest Proper TIF District
The Hazel Crest Proper TIF District could provide financing options for potential redevelopment and transit-oriented projects.
- Access/Visibility to Interstate 80/294 Expressway
Properly oriented along 171st Street, signage can be seen by more than 80,000 cars per day using Interstate 80/294. That visibility can improve the opportunities for commercial development in the area. It can make a marginal site more valuable than another similar site without this visibility. It is also possible for travelers, commuters, and residents to easily exit and enter the expressway in the study area.
- Institutions
There are public schools, private schools, churches and veterans' clubs located within the study area. These uses regularly bring potential customers to the area and provide a higher quality living environment.
- Temporary Closing of 159th Street
Few roadways cross the Canadian National Railroad Yard and the railroad tracks entering and leaving this yard. During the reconstruction of 159th Street, a major roadway crossing these tracks north of Hazel Crest, traffic will be rerouted. This presents an opportunity for local businesses to capture a portion of the increased traffic that will likely pass through Hazel Crest.



Some older structures have architectural character and charm.



Situation Audit

- Potential for Increased Density
Recent projects near commuter train stations in Matteson and Richton Park have sold very well. These successes suggest that similar increased residential density in the form of condominiums and senior housing may also work near the Hazel Crest Metra Station. Residential development, and the potential for additional customers and transit riders, should be a key element of any transit-oriented development.

Weaknesses

- Constrained Market
Businesses prefer markets where there is the potential for the number of customers, to increase. Hazel Crest Proper must demonstrate the ability to attract from outside the area or to increase the density of the area.
- Small, Shallow Lots
New development is more difficult in areas where ownership is highly fragmented. Assembling a parcel large enough to accommodate on-site parking may require complicated negotiations with multiple owners.
- Commuter Parking
Both municipal lots are at capacity, with waiting lists suggesting additional parking demand.

Threats

- Crime
Whether real or perceived, a fear of crime causes customers to avoid a shopping destination. Interviews and community meetings have identified Hazel Crest Proper as a location where low pedestrian activity, and dark buildings make people feel unsafe. However, a positive to build on is a recent survey of commuters using the Hazel Crest Metra station that indicates they do not share this perception.
- Aging/Deteriorating Buildings
Aging buildings can be charming or threatening depending on their original appearance and upkeep. In Hazel Crest Proper, 81% of the study area buildings are over 35 years old. Although the structures are kept in compliance with building codes by the Village's strict enforcement policies, owners have not replaced obsolete structures or remodeled the older structures with the potential to add charm and uniqueness to the area. The result is an image of decline that can discourage potential investors. Few of the existing buildings exhibit significant architectural character. Building rehabilitation assistance may be available through the TIF District to improve existing building conditions.



Existing commercial center.



Situation Audit

- Financing “Gap”
When prevailing market rents are below the rents necessary to provide a reasonable return on new construction, there is a “financing gap.” This gap means that new construction will only occur with public incentives. It also means that developers fail to add “extras” like enhanced facades and landscaping because it is not competitive to charge higher rents to support those improvements.
- “Central Place”
The best commercial development has a central place from which stores and amenities radiate. Without a central place, an area lacks the landmark that helps users identify it and has a disorganized, unplanned image. Hazel Crest Proper has such a space in Bicentennial Park. The plan recommends modification to the park to enhance this function and to give the park a fresh and inviting appearance.
- Need to Rebuild “Critical Mass”
The best convenience shopping areas offer the opportunity to easily complete a variety of tasks. For example, a resident can deliver a car for quick maintenance and, while waiting for the job to be completed, drop off dry cleaning, purchase greeting cards, buy dessert and linger over refreshments at a coffee shop. When a variety of businesses are close enough to offer this shopping opportunity, an area is said to have critical mass. Until this critical mass is recreated in Hazel Crest Proper, area businesses will have lower sales and be tempted to move to areas with better convenience shopping opportunity.
- Crosswalks
Well delineated, pedestrian-friendly crosswalks exist at the intersections of 170th Street with Dixie Highway and Wood Street. The crosswalks at the intersections of 171st Street with Wood Street and Park Avenue and the intersection of 170th Street and Park Avenue are poorly delineated. Improvements should be made to these intersections to facilitate pedestrian access to the Metra station. Such improvements are planned in the upcoming viaduct improvements.
- Aesthetic Issues
The elevated rail line provides excellent visibility into Hazel Crest. The lack of landscaping on the railroad embankment north of the Metra Station is also currently a negative for the area. However, with appropriate landscape improvements and proper maintenance, this threat can be converted into an asset.



Situation Audit

- Closing of Park Avenue
Park Avenue is currently closed south of 171st Avenue in East Hazel Crest. East Hazel Crest has expressed interest in having Park Avenue north of 171st Avenue closed through Hazel Crest. The Village of Hazel Crest feels that closing this road would severely reduce access to the Hazel Crest Station Area and future commercial and residential development.

Many of these items are illustrated on page 2-13 - Planning Issues.

TRANSPORTATION

Introduction

Addressing the relationship between the transportation system and the surrounding land use is fundamental to this plan, with the underlying principle that each mode and user is not separate unto itself, but is integrated in a system that is functional, yet interconnecting. All modes and users are included: automobile drivers, Metra commuters, pedestrians, Pace bus riders, and bicyclists.

Existing Roadways

The study area is served by five primary roadways: 170th Street, 171st Street, Wood Street, Park Avenue, and Dixie Highway. Each is described below. Recent traffic volumes are shown on page 2-12.

170th Street. This street functions as a collector street, providing access to the neighborhood businesses and residents, while also providing an east-west connection between Dixie Highway, Park Avenue, and the Metra station. This street is designated as Federal Aid Urban Route 1613 (FAU-1613). It has one lane in each direction with a parking lane on both sides of the street. The posted speed limit is 25 mph and there is a five-ton weight limit. 170th Street provides the core roadway to the study area. This road is under the jurisdiction of the Village of Hazel Crest. It has the potential to be a pedestrian friendly commercial and residential corridor. The Metra station is positioned at the terminus of 170th Street.

171st Street. This street is an east-west arterial route designated as Federal Aid Urban Route 1614 (FAU-1614). It is a two-lane roadway with one lane in each direction with turn lanes at major intersections. Four railroad bridges cross over 171st Street between Park Avenue and Ashland Avenue, providing the only east-west connection between 159th Street to the north and Dixie Highway to the south. The vertical clearance of these bridges range from 12'-1/2" to 12'-2". Parking is not permitted along 171st St. The pavement and sidewalks under the railroad viaducts are in very poor condition.



Situation Audit

171st Street carries about 18,000 vehicles per day. This roadway is the primary point of access for travelers, residents, and commuters from the east as it is the only road that crosses the Canadian National Railroad Yard for two miles north or south. 171st Street is also the primary collector roadway providing access to a considerable amount of high density residential development along I-80/294. The roadway is under the jurisdiction of Cook County. A jurisdictional transfer is under discussion for the segment from Park Ave. to Dixie Highway, changing from Cook County to the jurisdiction of the Village of Hazel Crest. A Project Development Report was completed in 2001 for 171st St. between Wood St. and Ashland Avenue. The preferred improvement alternative maintains the intersection of 171st St. and Park Ave. (north leg only). This is consistent with Consultant recommendations suggesting a need to keep Park Avenue open to support access to existing and future businesses.



The elevated Metra Electric line provides excellent visibility into Hazel Crest.

Currently the stacking of traffic, particularly truck traffic, to turn southbound on Wood Street from westbound 171st Street exceeds existing turn lane lengths and causes traffic to back up into the 171st Street and Park Avenue intersection. Alternatives for alleviating this condition can be found in the Truck Traffic Analysis contained in the Appendix.

Wood Street. This street is a north-south arterial that is two lanes in each direction. South of 171st Street, Wood Street becomes Governors Highway. Access to the I-80/294 entrance ramp is located just south of 171st St. additional signage is needed to direct motorists to the entrance ramp. This road is under IDOT jurisdiction. North of 171st Street, Wood Street has average daily traffic volumes (ADT) of about 11,400. ADT south of 171st Street is about 14,600.

Park Avenue. This collector road is configured in a general northeasterly direction and provides two lanes in each direction. The posted speed limit is 40 mph. Left turn movements from 171st Street onto northbound Park Avenue are not allowed due to the limited sight distance associated with the Metra Electric Railroad bridge. This road is under the jurisdiction of the Illinois Department of Transportation (IDOT).

Park Avenue carries about 4,400 vehicles per day. The intersection of 171st and Park has been identified as a high accident location in a 2001 Project Development Report by Robinson Engineering. Park Avenue provides access to the Hazel Crest Metra Electric station, however there is currently no direct drop-off zone. This is particularly significant because survey results indicate that a large number of users of this Metra station are dropped off. Sidewalks are not present along Park Avenue north of 169th Street. The south leg of Park Ave., across 171st St., has been vacated and has been converted into a commuter parking lot in East Hazel Crest.



Situation Audit

167th Street. This street is located at the far north of the study area. It is an east-west arterial with two lanes per direction. This road is under Cook County jurisdiction. 167th Street provides access to Interstate 57 west of Hazel Crest via a full interchange, and terminates into Park Avenue.

Dixie Highway. Dixie Highway is a north-south arterial with two lanes in each direction. It is under IDOT jurisdiction. Existing ADT is approximately 15,000. It provides access to the area for travelers, residents and commuters exiting eastbound I-80/294.

Metra Parking Data

Two surface lots provide parking for Metra commuters. About 70 percent of the parking spaces are permit spaces, with the remainder daily fee spaces. Metra parking is generally 100 percent occupied. A new commuter lot, in the Village of East Hazel Crest, is at Park Ave. south of 171st St., that includes 500 additional parking spaces.

Metra has prepared ridership and parking forecasts for the year 2020. Metra anticipates needing an additional 600 parking spaces by the year 2020 in this fare zone in addition to the new parking area in East Hazel Crest.

Lot	Location	Capacity	Usage
1	Park Ave./170th Street	94 (3 H/C)	85
2	170th Street/Wood Street	50	50
Total		144	135

Source: Metra 1999 Station Inventory, Metra Office of Planning Analysis

Private Parking

An inventory of existing parking conditions was conducted on a block-by-block basis within the study area, including both on- and off-street parking spaces. The illustration in the Appendix (A-16) shows the area included. Within the study area there are approximately 119 on-street parking spaces and 182 off-street parking spaces (excluding 144 spaces designated for Metra commuter parking). This total also includes parking spaces reserved for private residential use. The public on-street spaces experience an A.M. parking occupancy of 27 percent and a P.M. occupancy of 38 percent. Along 170th Street, parking is typically limited to 4 hour parking, Monday through Friday, 8AM – 4PM.

The off-street spaces, including both public and private, experience an A.M. occupancy of 66 percent and a P.M. occupancy of 64 percent. This higher off-street occupancy is attributed to the Metra commuter lots generally being filled, plus a few private lots designated for residents. Otherwise, the off-street lots would be only half filled, at best. The block-by-block occupancies for on-street and off-street parking spaces are shown in the Appendix.



Situation Audit

Metra Commuter Rail Service

The Hazel Crest Metra commuter rail station, located at the northeast corner of 171st St. and Park Ave., accommodates approximately 700 boardings per day and averages about 3,600 weekly boardings.

Year	Daily Boardings
1983	610
1985	740
1987	792
1989	779
1991	817
1993	802
1995	772
1997	675
1999	697

Source: Metra Boarding/Alighting Counts, 1999

About 90 percent of the daily boardings occur in the peak period and in the peak direction. The heaviest boarding occurs in the A.M. peak period at 6:54, 7:09, and 7:24. The heaviest volumes exiting the train in the P.M. peak occur at 5:22, 5:37, and 5:52. The above table presents existing and historical ridership data for the Hazel Crest station.

In 1999, Metra completed a system wide "on-board" survey, with one of the questions designed to address "Mode of Access" to determine how commuters get to their boarding station. As shown in the table below, more train riders (58%) drive alone to the station in Hazel Crest than at a typical station elsewhere in the Metra system.

Station Access	Hazel Crest Station	Electric Line Avg.	Metra System Avg.
Drove Alone and Parked	58%	40%	54%
Walked	14%	42%	23%
Dropped Off	14%	9%	13%
Carpool	6%	4%	4%
Took Bus	4%	4%	4%
Rode Bike	0%	0%	1%
Other	4%	1%	1%

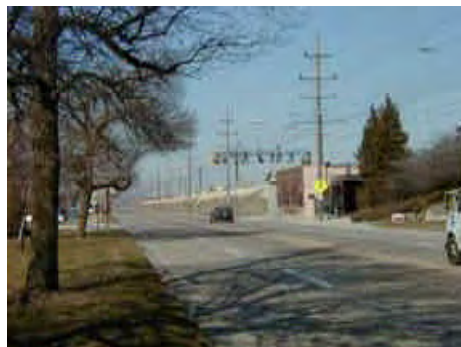
Source: Metra On-Board Survey, 1999

Pedestrian/Bicycle

All streets are shared with pedestrians and bicycles. Safe and efficient pedestrian and bicycle linkages are critical. The major barrier to an effective pedestrian and bicycle environment are the



Canadian National Railroad Yard Viaduct.



Park Avenue



Situation Audit

viaducts located at 171st Street. The commuter parking lot south of 171st St., in East Hazel Crest, creates the potential for significant conflicts between commuters and vehicles, particularly due to the limited sight distance created by the viaducts. Commuters will need to cross at the traffic signal at Wood Street and 171st Street when the viaduct improvements are completed. Sidewalks are present throughout the study area. With improvements to the streetscape environment, the pedestrian and bicycle system can be improved overall for a more safe and aesthetically pleasing experience.

Pace Bus

Two Pace bus routes serve the Hazel Crest Metra station or nearby area– Routes 352 and 460. These routes are indicated on the Planning Issues Map found at the end of this section. Route 352 is a regular, fixed route operating from the CTA 95th St. rapid transit line station to 16th and Vincennes (Chicago Heights terminal). It operates from about 4:20am until 12:45am, providing service to 170th and Wood Street every 15 to 30 minutes. Route 460 provides feeder bus connections directly to the Hazel Crest Metra station during peak hours. Service is provided via five AM peak period trips and four PM peak period trips. Service operates between 175th Street and Anthony Street in Country Club Hills to the Hazel Crest Metra station. Currently (May, 2002) average daily ridership is 54. This is down from 2000, when average daily ridership was 85 riders.

Issues

Based on the analysis of existing conditions, the following circulation, parking and transit issues are recommended to be addressed:

- 171st Street – viaduct improvements and impact on pedestrian connections
- Need for additional commuter parking to accommodate future Metra ridership; joint use opportunities should be explored with area churches and businesses focused on evening and weekend activity
- Accommodate future parking and traffic development
- Bus drop-off and Kiss-n-ride location
- Streetscape/improvements along Park Ave. to enhance pedestrian safety and appearance
- Pedestrian safety crossing 171st Street between the station entrance on the north side of the street and the new commuter parking lot in East Hazel Crest on the south side of the street.



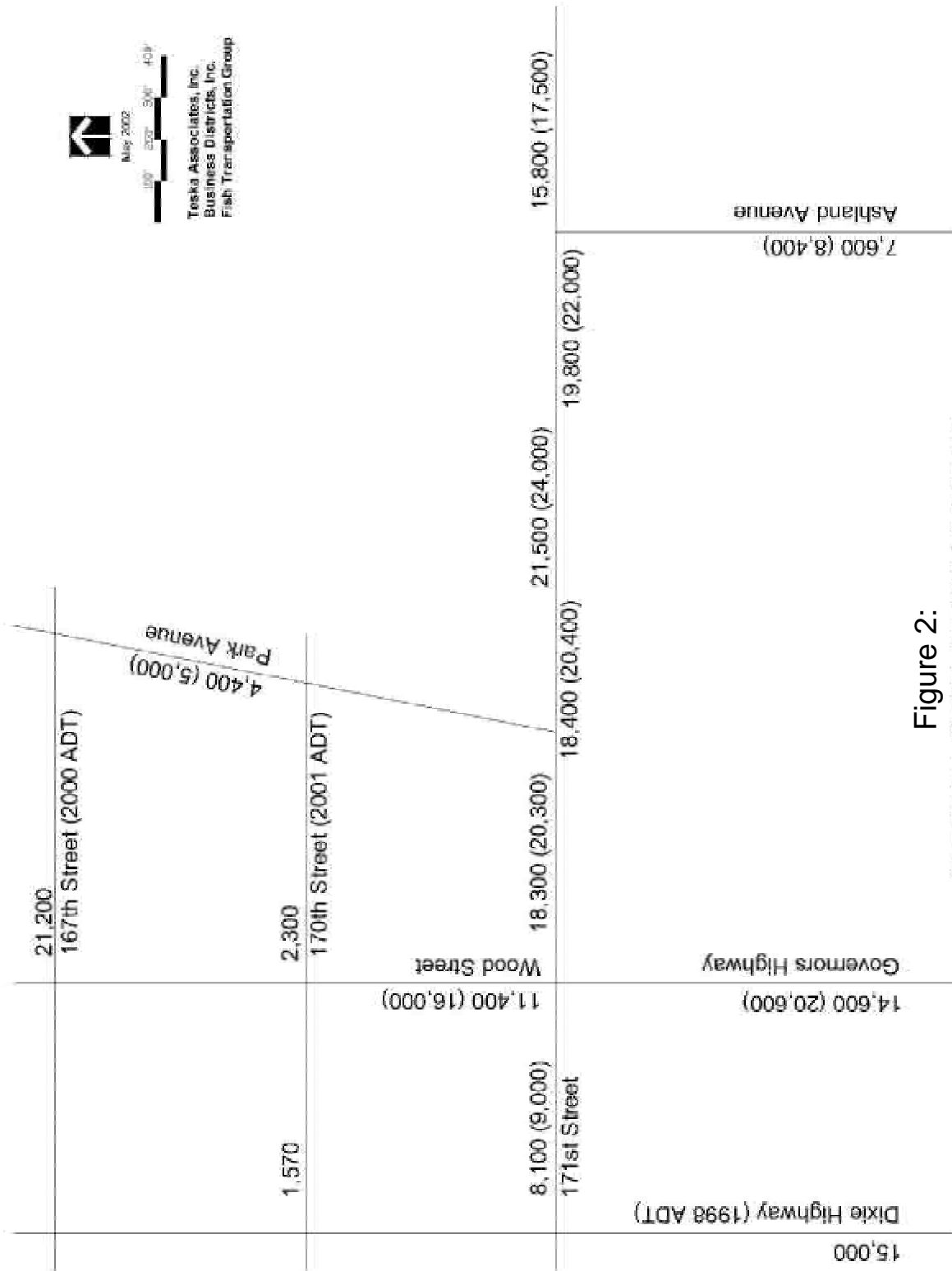


Figure 2:
Average Daily Traffic Volumes
2000 (2020)



PLANNING ISSUES

Hazel Crest Transit Oriented Development

Figure 3



LOCATION C



LOCATION B



EXISTING LAND USE

- SINGLE FAMILY RESIDENTIAL
- MULTI-FAMILY RESIDENTIAL
- COMMERCIAL
- INDUSTRIAL
- PUBLIC USE
- PUBLIC PARKING
- PARK

SPECIFIC ISSUE AREAS

- KEY PEDESTRIAN CROSSINGS
- SIGNALIZED INTERSECTIONS
- STATION ENTRANCE
- PACE BUS ROUTES
- TIF DISTRICT BOUNDARY
- VILLAGE BOUNDARY



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Redevelopment Strategy

A Concept Master Plan has been developed to summarize development/redevelopment recommendations within the study area. This section summarizes key recommendations for each site. Sites identified for redevelopment are sites originally proposed in the TIF Plan, along with additional sites identified during creation of this study. As such, the numbering of sites appears somewhat random. It was decided to maintain consistency with the site numbering in the TIF Plan, and to add additional sites to describe other redevelopment opportunities. Sites 15-22 are new redevelopment sites, not included in the original TIF Study.

Opportunity Sites

Opportunity Site Number	Existing Zoning	Proposed Zoning	Existing Use/Location	Future Land Uses	Redevelopment Priority	Site Area	Development Potential
1	B-1	R-3	Residence 2020 W. 170th St. Vacant Land Food and Liquor Store 2014 W. 170th St.	Residential - Townhome, duplex or single family	B	13,125	5 townhomes
2a	B-1	R-3	Residence 2019 W. 170th St.	a.) Residential - Townhome, duplex or single family	A		5 townhomes
2b	B-2	B-2	Lisa's House of Beauty 2011 W. 170th St.	b.) Retail / Office	A	31,388	4,600 sq. ft.
2c			For Sale 17000 Dixie Used car lot, office, contractor yard	a) Retail b) Bank or office	B	29,832	12,000 sq. ft.
3a	B-1	B-1	Joy's Hair Salon 1939 W. 170th St. Divas 1937 W. 170th St. Parking Lot 1927 W. 170th St. Rodney Brown 1925 W. 170th St.	a.) Retail or Bank b.) Office c.) With Second Story Residential	A	20,714	11,500 sq. ft. 3,000 sq. ft.
3b	B-2	B-2	Boz's Hotdogs 17031 Dixie			9,238	
4	B-2	B-2	Boz's Hotdogs 17031 Dixie Parking Lot - South Suburban Professional 17084 S. Winchester South Suburban Professional Center 17065 Dixie Office Building 17065 Dixie	Retail and Office	B	81,360	9,600 sq. ft.
5	B-2 R-2 S	Rezone only if School site is to be redeveloped	Hope Development Business Center 16901 Dixie 6 Single Family Homes Warren Palm School 1910 W. 170th St.	a.) School, office and residential b.) Community Center/Residential c.) Townhomes/ Commercial	C	157,949	64 townhomes
6	B-1	B-1	Vacant Land 1922 W. 170th St.	a.) Commercial b.) Office c.) With Residential Second Story	C	2,500	2,230 sq. ft. & 2 apartments
7	B-1	B-1	Residence 1906 W. 170th St.	a.) Office b.) With Second Story Residential	C	8,261	3,000 sq. ft.
8	B-1	B-1	Vacant Post Office 1822 W. 170th St.	a.) Day Care Center b.) Office	A	13,323	reuse of existing building
9	B-1	B-1	Vacant Land 1823 W. 170th St.	a.) Office b.) Residential - Multiple Family	A	5,805	3,000 sq. ft.



Redevelopment Strategy

Opportunity Sites (Cont.)

10A	R-2	B-2	Parking Lot 17034 Page	a.) Restaurant/ Retail	B	42,282	26,100 sq. ft.
			American Legion 17034 Page	b.) Community Center/Retail			
			Parking Lot 17068 Park	c.) Residential - Multiple Family			
10B	B-2	B-2	Residence 17031 S. Wood	a.) Commercial	C	45,590	77 parking spaces
			Residence 17043 S. Wood	b.) METRA Parking/ Commercial			
			Residence 17045 S. Wood	b.) METRA Parking/ Multiple Family			
			Residence 17055 S. Wood				
			Residence 17059 S. Wood				
			Residence 17069 S. Wood				
Residence 17073 S. Wood							
11	B-2	B-2	Autos in Motion 17082 Page	Auto Service	A	14,992	2,725 sq. ft.
12	R-2	B-2	Residence 16953 Page	Expanded METRA Parking	C	19,282	200 total parking spaces
			Residence 16957 Page				
			Residence 16963 Page				
13	B-2	R-3	Existing Metra Building	Residential-Multi-Family	B	46,650	80 condominiums/apartments if redeveloped
			Residence 1709 W. 169th				
14A	M-1	R-3	Contractor's Yard 16820 Park	a.) Commercial - Grocery Store b.) Residential - Senior Housing	A	85,424	120 condominiums
14B	B-2	R-3	Residence 16890 Park	a.) Commercial	C	34,874	80 condominiums
			Fast Security 16842 Park	b.) Residential - Multiple Family			
			Sign Shop 16852 Park				
15	B-2	B-2	vacant	office	B	17,178	6,000 sq. ft.
16	R-3	B-2	vacant	a) commercial	A	35,398	12,000 sq. ft.
				b) office			
17	R-3	B-2	Residence	Condominium with	A	26,474	6,000 sq. ft. and
	B-2		Office	ground floor retail			20 Multi-family units
18	B-1	B-1	Beef Hut	Restaurant/retail	B	5,739	5,300 sq. ft.
19	B-1	B-2	Beauty Shop on ground floor/	Condominium with		B	14,761
			apartments above	ground floor retail	20 Multi-family units		
20	R-2	R-3	vacant	townhomes	A	80,083	33 townhomes
21	R-2 special use	R-2 special use	church	church and parking	C	150,899	334 parking spaces
22	R-1	B-2	Residence	office	C	13,817	2,500 sq. ft.

New Commercial 113,055 sq. ft.
 New Townhomes 43 to 107
 New Multi-Family 285 to 322

Dixie Highway Corridor

Dixie Highway is a primary entry into the station area. Travelers going eastbound on I-80/294 can exit at Dixie Highway. Key features of this section of the plan include:







Sites 1 and 2a

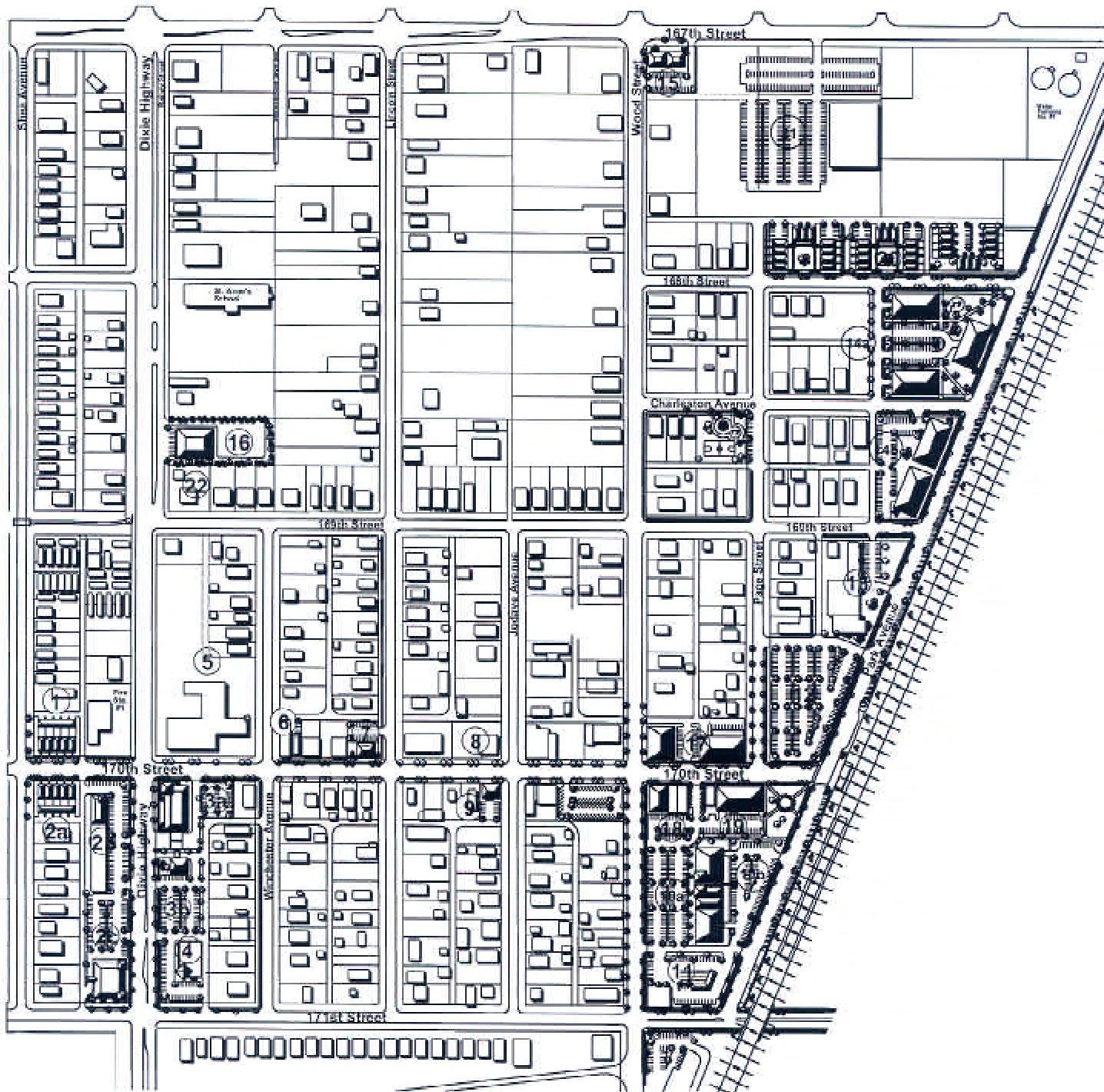
The area west of the alley running along Dixie Highway is almost exclusively residential. The current exception is the convenience/liquor store on site 1 and the former beauty shop on site 2a.





Legend

- | | | | | | |
|--|------------|---|--------------|---|--------------------|
|  | Commercial |  | Multi-Family |  | Redevelopment Site |
|  | Mixed Use |  | Town House |  | Way-finding Sign |



KEY	Use	Units	Square Feet	Parking Spaces
(16)	Townhouse	5	-	10
(17)	Townhouse	5	-	10
(18)	Commercial	-	4,600	30
(19)	Commercial	-	12,000	63
(20)	Commercial	-	11,500	76
(21)	Commercial	-	3,000	38
(22)	Commercial	-	9,000	65
(23)	School	-	-	-
(24)	Res/Retail	2	2,200	4
(25)	Commercial	-	9,000	20
(26)	Day Care (PO)	-	-	-
(27)	Commercial	-	3,000	12
(28)	Commercial	-	26,100	90
(29)	Metra Parking	-	-	77
(30)	Auto Service	-	2,725	40

(31)	Metra Parking	-	-	200
(32)	Metra Building	-	-	28*
(33)	Condominium	120	-	180
(34)	Condominium	80	-	120
(35)	Commercial	-	6,000	14
(36)	Commercial	-	12,000	11
(37)	Condo/Retail	20	6,000	80
(38)	Retail	-	5,300	20
(39)	Condo/Retail	20	6,000	33
(40)	Townhouse	53	-	85
(41)	Existing Church	-	-	-
(42)	Existing Use	-	-	-
City Parking at Wood and 170th				50
Metra Parking Along Park Ave.				200
Total		285	115,085	1,563

Total Metra Parking - 541

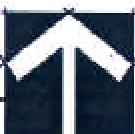
*Only 14 of the 28 spaces are available for commuter parking



Transit Oriented Development Plan - Draft
Village of Hazel Crest
 Overall Development Concepts

Figure 5

July 2009



Redevelopment Strategy

While the convenience store provides needed support for the neighborhood, it is recommended that this use be relocated along Dixie Highway to minimize intrusion into the neighborhood. Both sites 1 and 2a should ultimately be redeveloped for residential use. The Concept Plan shows possible redevelopment for townhomes, with five dwelling units suggested for each of the two sites.

Site 2B

Site 2B is proposed for extension of the existing strip shopping center on the west side of Dixie Highway. The corner of the building should have some unique architectural element that addresses the corner. To minimize traffic conflicts at the intersection, it is recommended that site access be from 170th Street. This access should be back from Dixie Highway to provide appropriate space for car stacking.

Site 2C

This site is at a key entrance to the study area, in a location that is visible from the interstate. The area currently contains a used car business and a heating contractor. The heating contractor has expressed interest in expanding to a new location within the study area. When this occurs, it would be appropriate to consider the entire area for redevelopment. The concept plan proposes a bank or perhaps a pharmacy with a drive-through located on the corner to provide a strong visual anchor to the corner and set a positive tone for the rest of the study area. Parking then would be located to the north, allowing for a possible sharing of parking spaces and cross-access with the adjacent strip shopping center.

Site 3a

Located at the southeast corner of Dixie Highway and 170th Street, this site currently contains several small businesses and apartments. However, buildings are located too close to the corner, limiting sight distances and requiring placement of traffic signals too close to the roadway. This area is suggested for ultimate redevelopment. The concept plan suggests a bank with drive-through service. A bank would provide needed services to area residents while providing a strong visual and physical anchor for this key corner location. Given the sites visibility at a signalized intersection, other appropriate uses would focus on retail activities or a restaurant.

Site 3b

Site 3b is the mid-block area on the east side of Dixie Highway between 170th Street and 171st Street. This area currently contains a hot dog stand and parking for the adjacent office building. These uses are anticipated to continue. However, as illustrated on the Concept Plan, it is recommended that the hot dog stand be reconstructed, providing a drive-through and enhanced seating



Redevelopment Strategy

consistent with modern business practices in the industry. Landscape enhancements are proposed for all parking areas, including parking lot end islands to better define circulation and screening along Dixie Highway.

Site 4

Site 4 is currently occupied by an office building. Given this site's excellent visibility from I-80/294, it would make an excellent redevelopment site for a pharmacy or other retail business. A restaurant would be another appropriate business for this key location.

Site 5

Warren Palm School covers most of this block, with several single-family homes located along Winchester Avenue and an office building located at the southeast corner of Dixie Highway and 169th Street. The concept plan shows this block remaining untouched by redevelopment. However, Warren Palm School is currently vacant except for limited use as an administration center for the school district. Should the school district decide to vacate the property at some point in the future, several alternative redevelopment scenarios should be considered for this block. A possible reuse of the building could be as a community center. However, given the age and condition of the structure this may not be feasible. Private redevelopment is another possibility, either for commercial use or as a site for townhomes. A sketch of a possible townhome layout is shown in Figure 7. This plan features a neighborhood green along Winchester Avenue, internal circulation, and units that address the street and provide a pleasant streetscape environment. While this sketch illustrates redevelopment of the entire block, a more limited redevelopment would be possible which would preserve the existing homes along Winchester Avenue.

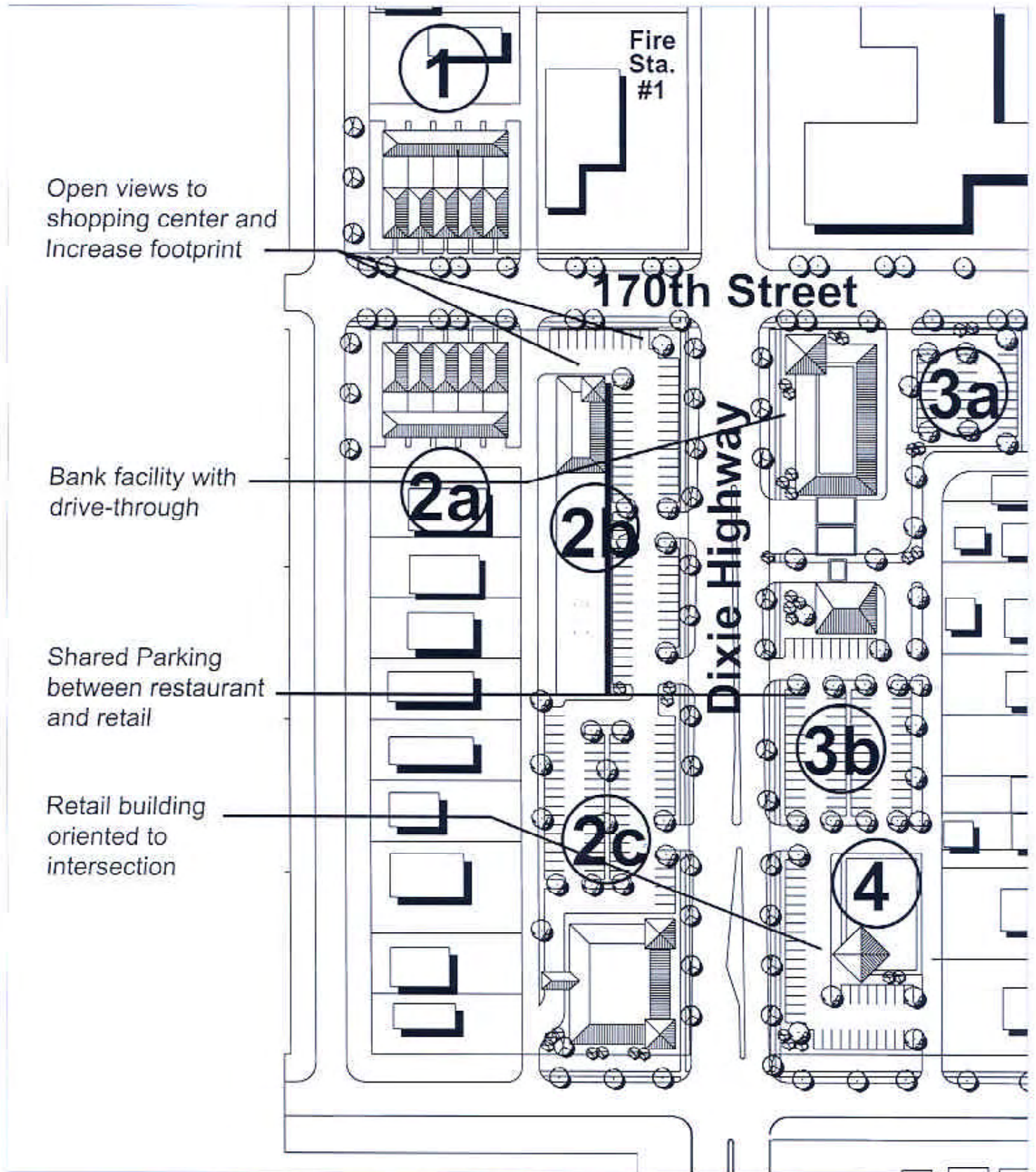
Site 16

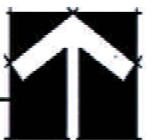
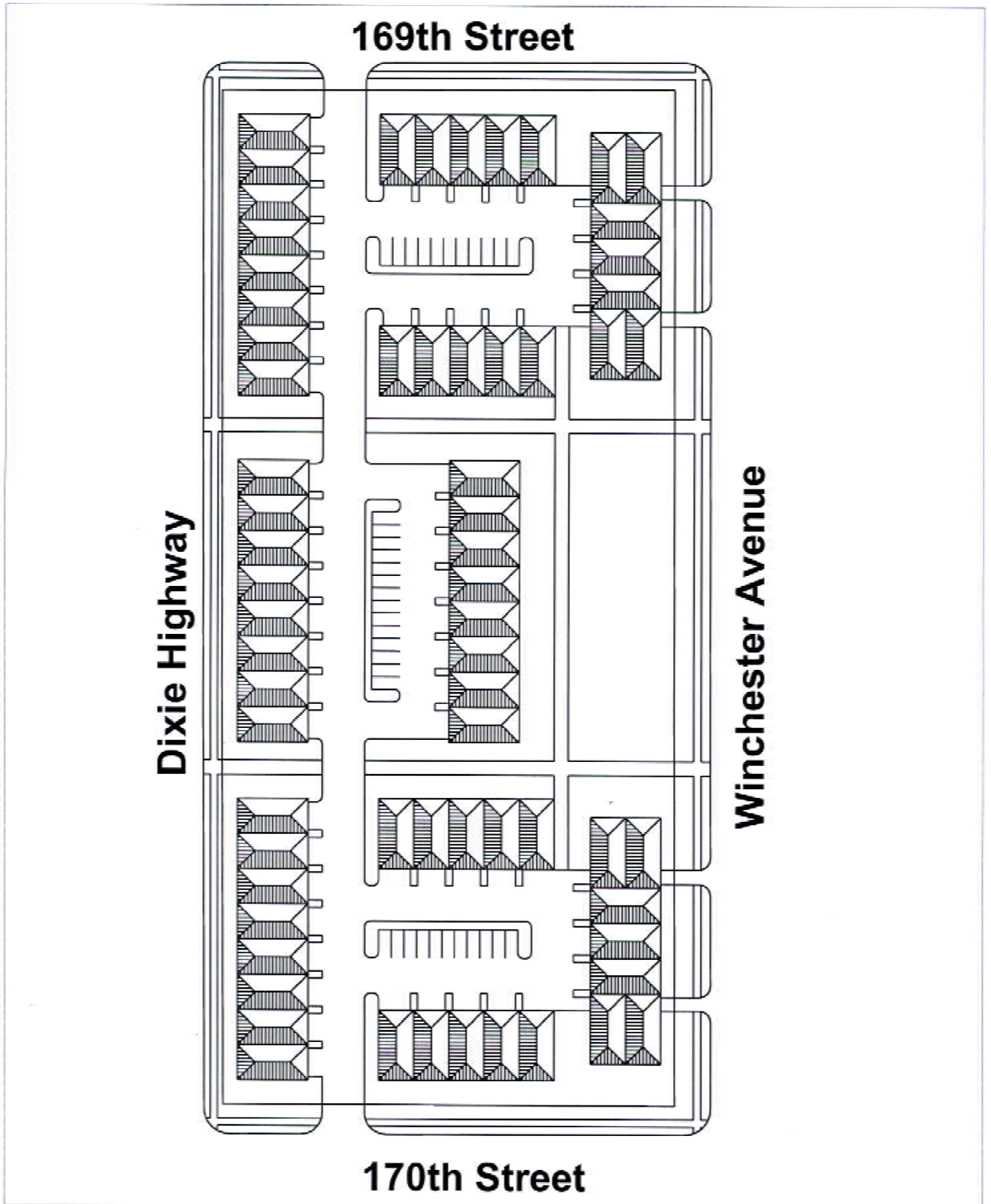
Located north of 169th Street on the east side of Dixie Highway, this parcel is currently vacant and owned by the Village of Hazel Crest. The concept plan illustrates redevelopment for a commercial business, with landscape buffering around the periphery of the lot to enhance the transition and compatibility with surrounding uses.

Site 22

Located at the northeast corner of 169th Street and Dixie Highway, this parcel is currently in use as a single-family home. Given the corner location and proximity to other business uses along Dixie Highway, the concept plan illustrates reuse of the property as an office. This reuse could occur within the existing structure, or the property could be redeveloped and a new structure constructed.







Redevelopment Strategy

170th Street Corridor

The 170th Street Corridor was the Village's "Main Street" at one time. While once predominately commercial, this corridor is now a distinctly mixed-use area, providing a key link between the immediate station area and the Dixie Highway corridor. The following redevelopment sites are recommended for the 170th Street corridor:

Site 6

Site 6 is a small vacant lot located on the north side of 170th Street just east of Winchester Avenue. Suggested development of this site is modeled after the adjacent property to the west, with ground floor business use and one or two apartments on the second floor. However, the footprint of the proposed building is smaller than the adjacent development to accommodate appropriate parking at the rear of the property.

Site 7

Located at the northwest corner of 170th Street and Lincoln Street, this site currently contains a small structure located near the alley but is very underutilized. Redevelopment for a small office, with associated parking, is recommended. The new building should be located near the street to match the character of adjacent structures.

Site 8

As the location of the former Hazel Crest Post Office, this site at the northwest corner of 170th Street and Jodave Avenue was once a center of activity within the community. A day care center is planned for this location. This type of activity is an appropriate reuse of this site, returning daily activity to the area. This reuse will provide needed services to both neighborhood residents and commuters. It will also provide additional visits to the area that may increase demand for adjacent businesses.

Site 9

Site 9 is a vacant property at the southwest corner of 170th Street and Jodave Avenue. This property was a neighborhood bar at one-time. Similar to Site 7, this property is proposed for a small office building located near the front property line. On-site parking is proposed for the rear of the property, accessed from the alley.

Station Area

The area immediately adjacent to the Metra station offers exciting redevelopment potential. This area should be a focal point of revitalization efforts, and should set the town for improvements in the overall Hazel Crest Proper neighborhood.





KEY	Use	Units	Square feet	Parking Spaces
1	Townhouse	5	-	10
2	Townhouse	5	-	10
3	Commercial	-	4,000	30
4	Commercial	-	12,000	63
5	Commercial	-	11,500	28
6	Commercial	-	3,000	38
7	Commercial	-	3,800	65
8	School	-	-	-
9	Res/Retail	2	2,230	4
10	Commercial	-	3,000	20
11	Day Care (PO)	-	-	-
12	Commercial	-	3,000	12
13	Commercial	-	26,100	90
14	Metra Parking	-	-	77
15	Auto Service	-	2,725	40
16	Metra Parking	-	-	200
17	Metra Building	-	-	28*
18	Condominium	120	-	150
19	Condominium	80	-	120
20	Commercial	-	6,000	14
21	Commercial	-	17,000	11
22	Condit/Retail	20	6,000	80
23	Retail	-	5,300	20
24	Condo/Retail	20	6,000	33
25	Townhouse	33	-	80
26	Existing Church	-	-	-
27	Existing Use	-	-	-
City Parking at Wood and 170th				50
Metra Parking Along Park Ave.				200
Total		285	113,065	1583
Total Metra Parking				541

* only 14 of the 20 spaces are available for commuter parking



Redevelopment Strategy

Site 10a

Located on the east side of Wood Street between 170th Street and 171st Street, this area currently contains several single-family homes. Given the sites proximity to the train station and good traffic counts along Wood Street, the site is recommended for redevelopment as a retail center. The concept plan shows a fairly traditional layout, with a double row of parking at the front of the stores that will face Wood Street. However, all sides of this retail center should be attractively designed, particularly the rear façade which will be visible from the adjacent commuter rail line. Signs should be permitted both along Wood Street and Park Avenue to take full advantage of the property's prominent location.

Pedestrian circulation within and for this retail center is also critical. Sidewalks should be provided to provide a convenient connection into the site from Wood Street, directly to the train station entrance at 170th Street and Park Avenue, and to the proposed commuter parking lot to the rear of the development along Park Avenue.

Site 10b

This currently contains several parcels, including the VFW, a small bar, and the existing Page Street right-of-way. While an important location given the site's proximity to the train station, it is likely that redevelopment of this property will not occur until the VFW decides to relocate or cease operation in this location or redevelopment of Site 10a occurs. In either case, the suggestion outlined in the Concept Plan calls for this property to ultimately be acquired by the Village and converted to commuter parking.

The parking lot should be designed to provide appropriate landscaping such as screening along Park Avenue and end islands to define the circulation system. The facility will also need to accommodate service to the rear of the proposed retail center along Wood Street.

Site 11

Site 11 will be significantly impacted by the planned 171st Street viaduct improvements. Due to the lowering of roadway grades under the viaduct, access to the property at the northwest corner of Park Avenue and 171st Street will be reduced. The existing corner structure must be removed to accommodate this construction. The concept plan calls for reuse of this site for auto service uses to complement the existing auto repair business located at the northeast corner of Wood Street and 171st Street (Topgun). This type of service business will be convenient for both residents and commuters.



Redevelopment Strategy

Site 12

An existing Village-owned commuter parking lot, along with three single-family homes are included in Site 12. This area is proposed for future expansion of the commuter parking lot. The existing lot is currently fully subscribed, with a waiting list. Additional parking in this key location will also be available at night and on weekends for nearby businesses along 170th Street. Landscaping should be provided within the parking lot and around the periphery of the lot.

Site 13

Site 13 contains a Metra storage facility and an adjacent single-family home. Given this site's proximity to the station, this property has significant potential for redevelopment. However, Metra's current plans are to maintain this property as a storage facility for the foreseeable future. The concept plan shows this facility remaining, with minor reconfiguration of the parking lot and landscape enhancements to improve the overall appearance of the facility. A sketch of recommended improvements is included. Should Metra ever decide to vacate this facility, redevelopment for multi-family housing is recommended. An alternative sketch of how this residential redevelopment could function is also included. This alternative features two multi-story residential buildings with either condominiums or apartments. These units would be in direct view from the train station, facing Park Avenue and the railroad. Parking would be both underground or on the first level, with the housing units above. Additional space for surface parking is also suggested.

Site 14a

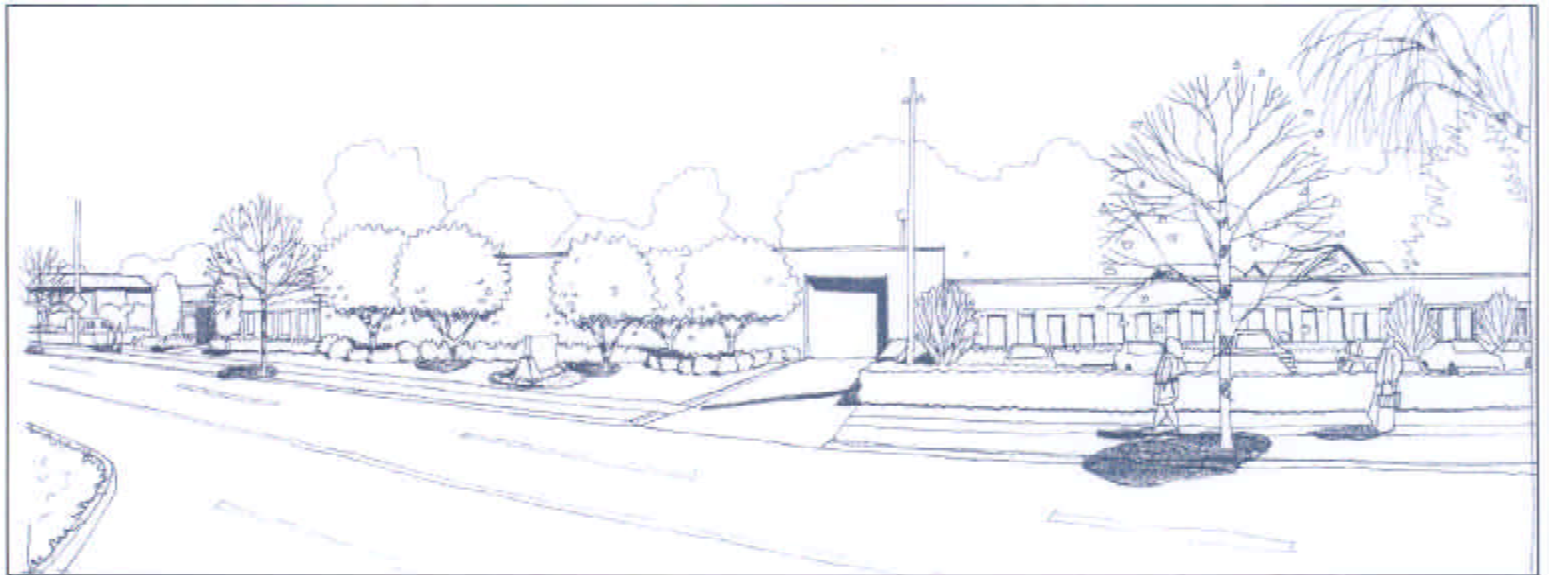
This site is partially used by a local contractor as a storage yard, but is largely vacant. The outside storage and general condition of the site detract from the overall appearance of the study area. Although townhomes were originally proposed for this site, the concept plan currently illustrates condominium development. This change is due to the sandy soils in the area, which suggest that a larger footing that would be typical of a multi-family structure would be more realistic. Visibility from the Metra rail line and Park Avenue are critical to development of this site. Buildings and open space should be oriented to the east of the property, with parking and vehicular circulation focused to the west along the Pualina Avenue right-of-way (no roadway currently exists in this right-of-way). Underground parking is also recommended under the building provided this is feasible given soil conditions.



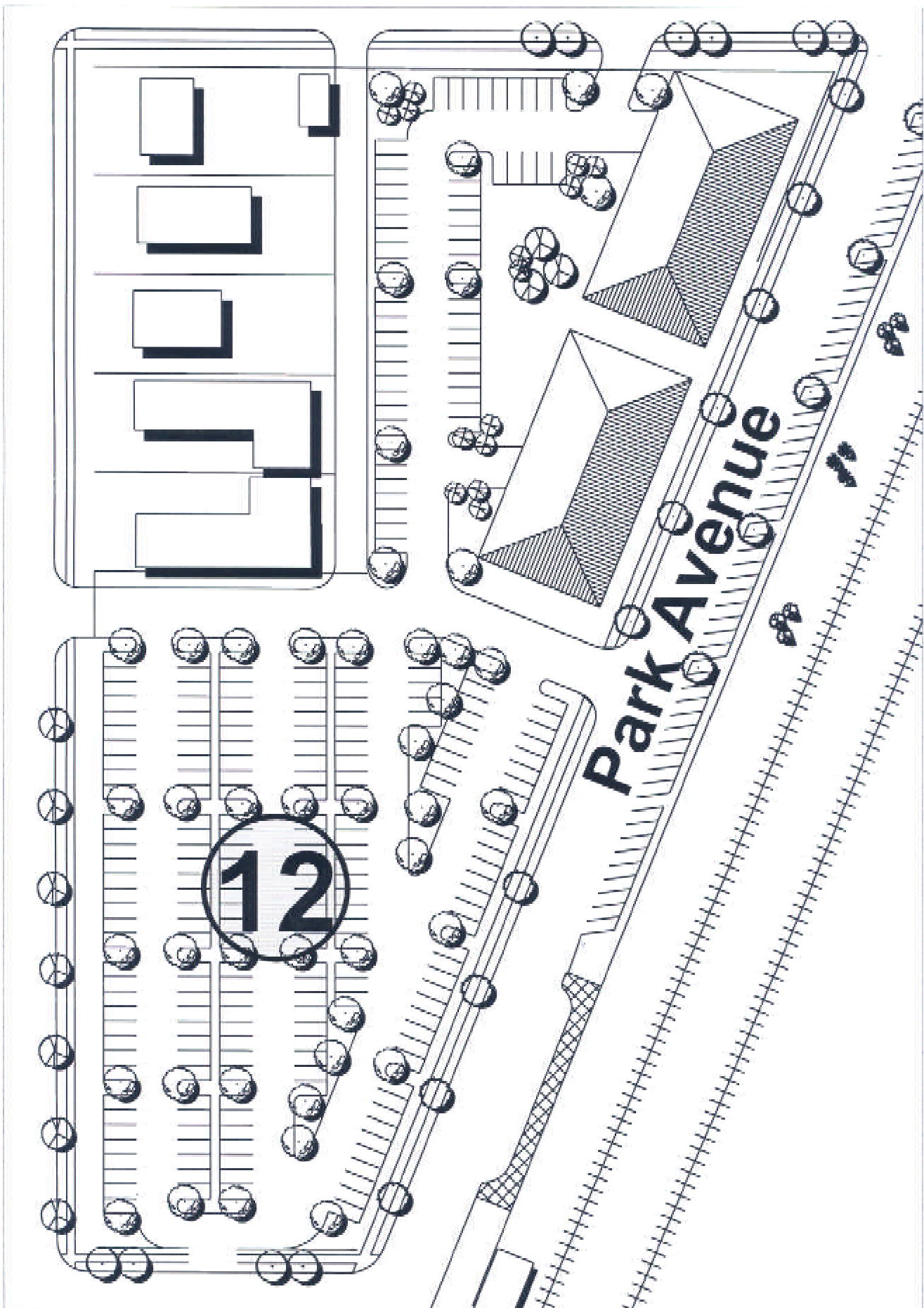
View of Proposed Landscape Improvements at Metra storage building



Existing



Proposed



Redevelopment Strategy

Site 14b

This triangular shaped property currently contains several small commercial businesses. Given the sites proximity to the station, the concept plan calls for the ultimate redevelopment of this property for multi-family residential use. The two buildings proposed could either accommodate apartments or condominium units. As with Site 14a, the vacation of Paulina Avenue is proposed. However, in this case the roadway does currently exist. A utility easement will be required for access when this vacation occurs. Buildings should be oriented to Park Avenue and the railroad, with parking to the west. Garage parking below the building is also recommended to enhance the marketability of the property.

Site 17

Site 17 comprises the frontage of 170th Street between Wood Street and Page Street. The area currently contains a mix of uses, including a single-family home, and office, and several apartments. The concept plan suggests future redevelopment of this block given the properties excellent vehicular and transit access. Three-story mixed-use development is proposed, with ground floor business use and upper story housing (apartments or condominiums). As an alternative, the building at the corner of Wood Street and 170th Street could be a freestanding retail or service business.

Buildings should be oriented to 170th Street, with little or no setback. Parking should be provided to the rear or in-between buildings. One level of parking is proposed under the structure for residents.

Site 18

Located at the southeast corner of Wood Street and 170th Street, this key corner is home to an established neighborhood business, the Oasis Beef Hut. This establishment is currently a carryout only business, with no on-site seating. Expansion of this business is recommended to provide indoor seating and enhance the appearance of this major intersection. These goals can best be accomplished through new construction.

Site 19

Site 19 refers to the existing Page Street right-of-way south of 170th Street. The concept plan indicates vacation of this right-of-way, with the land being shared by adjacent properties for reconfiguration of Bicentennial Park, commuter parking, and private development. A utility easement will likely be required prior to any right-of-way vacation to provide for continued maintenance of underground utilities.



Redevelopment Strategy

Site 20

This property, located on the north side of 168th Street, is predominately vacant and owned by the adjacent church. This site would provide another opportunity to provide housing within easy walking distance to the train station, and is recommended for townhome development. A courtyard design approach is suggested on the concept plan, with units clustered around common open space or parking courts. The development should have an attractive appearance from 169th Street, with parking and other service functions located to the north within the platted but undeveloped alley. Buildings should contain two to six units, and should have attached garage spaces and private patios or balconies. To enhance the marketability of the project, it is recommended that all townhome development occur in one phase.

167th Street Corridor

At the northern edge of the study area, two properties along 167th Street play a role in the overall redevelopment strategy.

Site 15

Site 15 is a vacant corner lot at the southeast corner of 167th Street and Wood Street. Reuse of this property as a freestanding office is recommended and illustrated on the concept plan. Expansion of the existing day care center, or other attractive commercial uses would also be appropriate for this site.

Site 21

Site 21 is a new church located along 167th Street between Wood Street and Park Avenue. This facility already has a large parking lot, and the property could accommodate additional parking facilities. While no redevelopment is proposed for this parcel, it has been included in the concept plan as an additional location for commuter parking. Such shared parking use is successful in other areas, and can provide revenue for the church while meeting commuter needs for convenient parking. The site has excellent vehicular access from 167th Street, and would be about a ¼ mile walk for commuters from the station. However, lots such as this, which do not have a direct visual link, or line-of-sight, with the station are often underutilized. As such, the option of promoting this site for commuter parking is only recommended after other suggested parking facilities are developed and fully utilized. In total, the concept plan illustrates 324 parking spaces on this site. Of this total, 250 spaces are estimated to be available for commuter parking in the future. If used for commuter parking, a shuttlebus might make this location more viable.



Redevelopment Strategy

Parks and Open Space

The existing Bicentennial Park is in a key location, directly across Park Avenue from the train station. Given this prominent location, it is recommended that the park be redesigned into a prominent public space with a strong, identifiable public focal point. The park should feature seating, attractive landscape enhancements, and other passive features that would be unique to this location and this neighborhood.

Through discussions with residents and the Park District, considerable interest has been expressed in development of a new small neighborhood park to provide residents with recreational opportunities without having to cross a major street such as Dixie Highway or 167th Street. Given the character and relatively small size of the neighborhood, the park would not need to be large. Recommended facilities include a playground, a basketball court and appropriate seating. Specific facilities should be determined by the Park District after meeting with the neighborhood to determine local demand. The only existing park in the neighborhood, Bicentennial Park, is not appropriate for this activity given its location and function as a passive open space adjacent to the train station.

Although illustrated on the concept plan at the southwest corner of Charleston Avenue and Page Street, this park could be located in a number of locations within the study area. Primary considerations should include:

- A location on a low traffic residential street
- A location within easy walking distance of existing and future residents
- Sufficient area to accommodate a basketball court and a playground
- Placement outside of the TIF District to avoid negative cash flows

Pedestrian and Vehicular Circulation

A number of pedestrian and vehicular circulation improvements are needed within the study area to improve overall safety and convenience. These improvements are summarized below.

Sidewalk Extensions

To facilitate access between proposed residential development and the station, new and/or improved sidewalks are needed along Park Avenue. These walkways will provide a safe means of circulation



Redevelopment Strategy

in the station area, as well as to the possible private commuter lot on the church property (Site 21).

Traffic signals at Dixie Highway and 170th Street

The Village has had significant problems with damage to traffic signals at the intersection of Dixie Highway and 170th Street given their close proximity to the roadway. With the redevelopment illustrated on the Concept Plan, it is recommended that signals should be setback farther from the roadway to enhance motorist safety and reduce the cost of signal replacement.

Way-finding Signs

Particularly after the closing of Park Avenue south of 171st Street, the Hazel Crest Metra Station is somewhat hidden from surrounding major roadways. A system of way-finding signs is proposed to direct visitors to area businesses and the station. Locations of proposed signs are illustrated on the Concept Plan by yellow diamonds. These locations are summarized below:

1. Intersection of Dixie Highway and 171st Street – This sign would welcome visitors to the Hazel Crest Proper area from north bound Dixie Highway.
2. Intersection of Dixie Highway and 170th Street – Two signs are proposed, one for north bound and one for south bound Dixie Highway motorists. Both signs would direct motorists to the station and area businesses.
3. Intersection of Wood Street and 171st Street – This sign would welcome motorists to the station area and direct them to commuter parking.
4. Intersection of Wood Street and 170th Street - This sign would also identify the station area and direct motorists to commuter parking.
5. 171st Street east of the viaduct - This sign would welcome motorists to the station area and direct them to commuter parking.
6. Park Avenue and 170th Street – This sign would be a pedestrian kiosk to inform commuters of neighboring businesses and available parking.

Mid-block West-East Access from Wood to Park Avenue

A critical link in the circulation system will be the proposed east-west access between Wood Street and Park Avenue just north of 171st Street. Given limited access at the Park Avenue, 171st Street intersection, this route will be needed to provide access to the



Entry Sign Example for Consideration Along Dixie Highway



Way-Finding Sign Example to Direct Motorists to Parking and Businesses



Redevelopment Strategy

proposed commuter parking along much of the length of Park Avenue in Hazel Crest. It may be appropriate for the Village to view this connection as a Village street, perhaps with a reduced right-of-way since sidewalks and utilities will not be required in this location. Given the proposed vacation of other road segments noted below, maintenance of this new roadway segment should not have a significant detrimental effect on municipal finances.

Page Street Vacation

To accommodate additional redevelopment, parking, and open space Page Street is proposed for vacation south of 170th Street. This stretch of roadway currently only provides direct access to one property, the American Legion building. This structure is proposed for ultimate removal in the overall concept plan. A utility easement will likely be required as a part of this vacation to insure continued access to existing utilities.

Paulina Avenue Vacation

Although the right-of-way for Paulina Avenue extends from 168th Street to 169th Street, only the southern half of this right-of-way was ever improved. The existing part of this roadway serves primarily as an alley access to properties which front on Park Avenue. This alley function is maintained in the concept plan, with a portion of the right-of-way dedicated back to accommodate expanded private development. As with the Page Street vacation above, a utility easement will likely be required as a part of this vacation to insure continued access to existing utilities.

Commuter Parking

Significant potential exists for increased commuter parking within the study area. Additional commuters will provide more potential customers for local businesses, help to reduce overall congestion on regional roadways, and help to support overall rail operations. The following table provides a summary of existing and proposed commuter parking within the station area.

Location	Existing Parking	Proposed Parking	Comments
170 th Street & Wood	50	50	Existing daily fee municipal lot
170 th Street & Park Avenue (Site 12)	94	200	Existing monthly municipal lot proposed for expansion
Page Street (Site 10b)	12	77	Existing private parking – proposed municipal lot
Metra Building	0	14	Metra owned facility - limited use of building suggests potential rental of a portion of existing spaces
Park Avenue	0	200	Requires IDOT cooperation to reduce travel lanes and add angle parking
Sub-Total	156	541	
Church Parking (167 th Street)	0	250	Possible private fee-based commuter parking – limited potential due to line-of-sight concerns
Total	156	791	



Redevelopment Strategy

In addition to the parking noted in the preceding table, additional commuter parking exists south of 171st Street in East Hazel Crest. This newly expanded lot provides continuous parking from the Calumet Station in East Hazel Crest to the Hazel Crest Station. Commuters parking at the northern end of this lot will be closer to the Hazel Crest Station and will likely use this facility. The concern will be provision of a safe crossing of 171st Street after viaduct and other roadway improvements are made near the intersection of Park Avenue. Based on the current proposal, pedestrians will need to go west to cross 171st Street at the signalized Wood Street intersection. Enhancement of the pedestrian crossing at Wood Street is needed to improve this movement.

Streetscape Enhancement

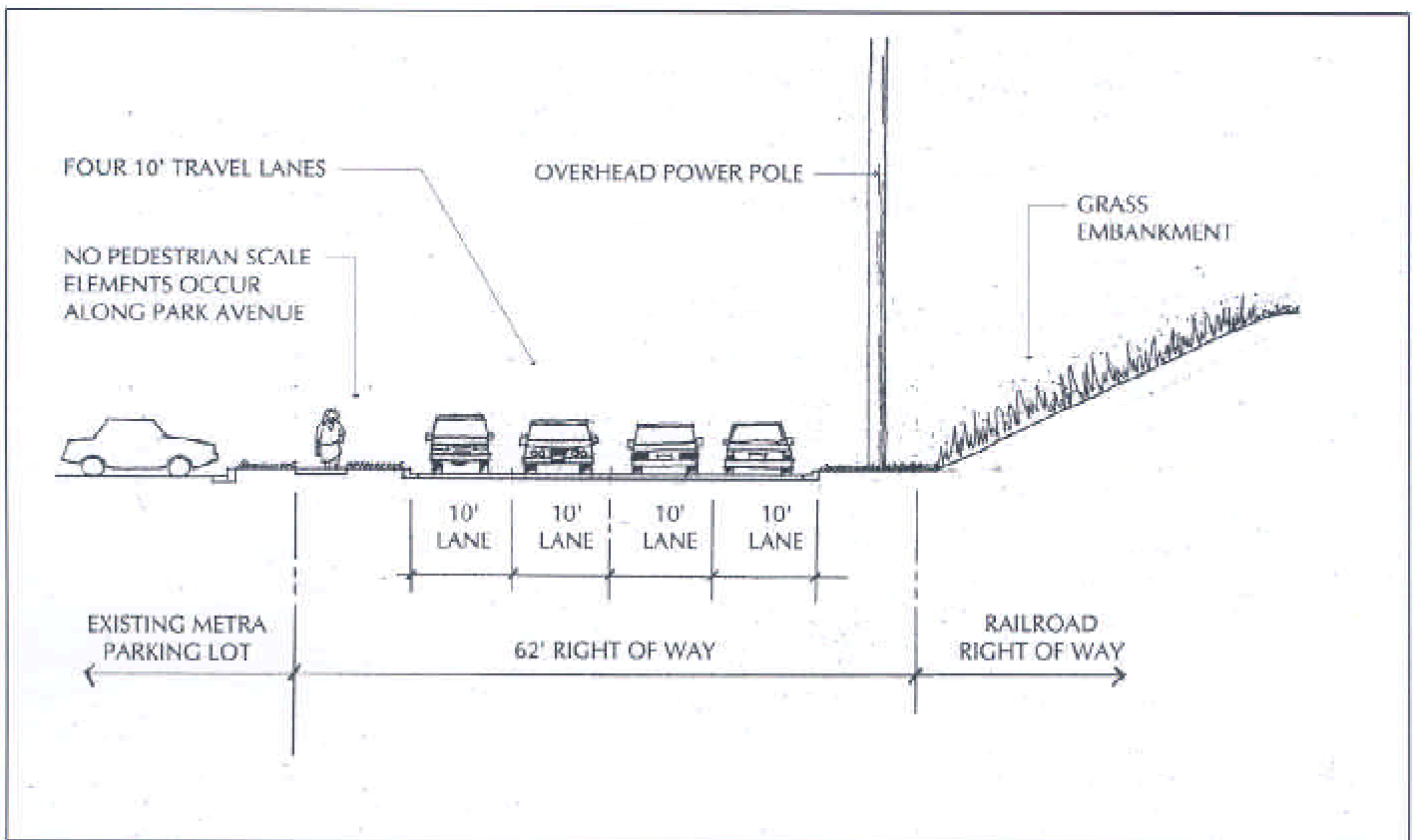
Both public and private sector improvements are needed to revitalize the study area. This section focuses on public sector improvements within existing roadway rights-of-way. The design guideline section provides recommendations for improvements on private property.

Park Avenue

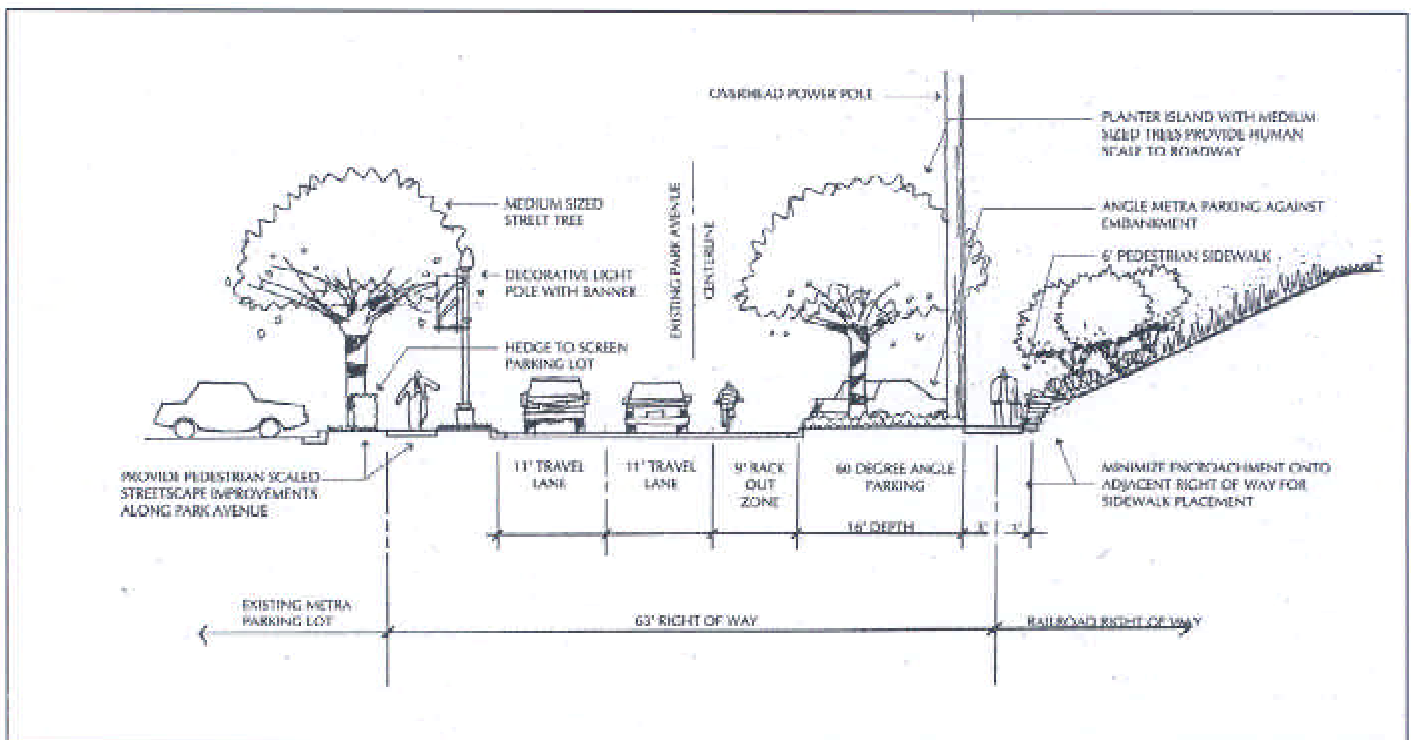
As a four-lane roadway, Park Avenue has considerable excess capacity that is likely to increase due to the recent closing of Park Avenue at 171st Street. Given needs for commuter parking and enhanced drop-off facilities at the station, significant modifications are recommended to this roadway profile. The Concept Plan highlights plans to provide significant commuter parking along the east side of Park Avenue, along with a passenger drop-off area and small plaza space near the station entrance. Figure 11 illustrates existing and proposed conditions. Since this is currently an IDOT controlled roadway, the Village will need to work closely to coordinate improvements both with IDOT and the railroad. Recommendations include:

- Approximately 200 parking spaces (60 degree angle)
- Reduction from 4 moving lanes to 2 moving lanes, with turn lanes at 170th Street
- Sidewalks on both sides of the street
- Decorative lighting and banners
- Landscape enhancement to the embankment north of the Metra station (south of the station is already well landscaped)
- Street tree planting
- Passenger drop-off area (Kiss-n-Ride)





Existing



Redevelopment Strategy

170th Street

The 170th Street corridor provides a critical east-west access into the station area for both pedestrians and vehicles. The corridor ties together the Dixie Highway corridor with the station area, and is the historical business district for the community. As such, it deserves an enhanced streetscape appearance worthy of its present function and historic value to the community. Highlighted in the illustration on the following page are the following features:

- Corner bump-outs with decorative paving and well marked crosswalks for enhanced pedestrian safety
- Grass parkways adjacent to residential development, featuring clustered street tree plantings and flowering trees near key intersections.
- Maintenance of parallel parking on both sides of the street
- Decorative, pedestrian scale streetlights with event banners
- Consistent street furnishings, including trash receptacles and benches near the east end of the corridor.

Wood Street

A simple, but more distinctive streetscape is also planned for the Wood Street corridor between 170th Street and 171st Street. The existing streetscape consists of little more than a concrete sidewalk and generic street lighting. The Concept Plan features decorative, pedestrian scale lighting with banners and street tree plantings.

Dixie Highway

A simple, but more distinctive streetscape is planned for the Dixie Highway corridor. The existing streetscape is very utilitarian, with parking lots paved to the curb line, limited lighting, and no banners. Both existing and proposed conditions are illustrated in Figure 12. The proposed plan features decorative, pedestrian scale lighting with banners and street tree plantings. Also illustrated is shrub screening of parking lots on private property, monument style signs, and more interesting building facades and features.



Design Guidelines

The Design Guidelines described herein are intended to be used as tools for communicating the design intent for future development and for evaluating development proposals. The overall goal is to ensure quality development that employs sound planning and design principals.

The purpose of these guidelines is not to dictate a specific development plan for the properties located in the Hazel Crest Proper area. Rather, these guidelines establish a set of standards and identify elements of building and landscape design that should be encouraged in this district.

The Design Guidelines promote a unique district by encouraging sensitive modifications to existing buildings and compatible design of new construction that enhances the context of the Metra Electric Station area. These guidelines are intended to promote the vitality and economic health of the station area by creating and enhancing its physical appearance and image as a desirable place to live and shop. The process of design review is intended to ensure that new development, redevelopment, and remodeling enhances the visual quality and identity of the station area.

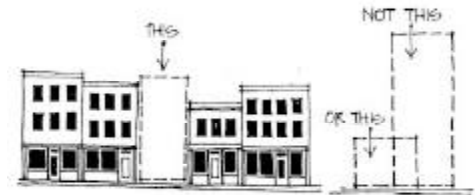
These design guidelines apply to all commercial and residential properties located in the Hazel Crest Project area.



Design Guidelines

General

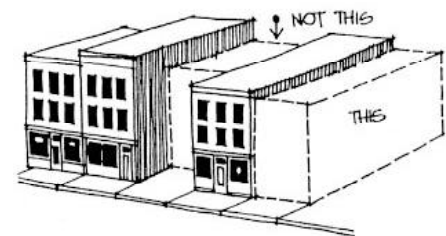
- Infill development along streets should maintain and reinforce the line of the storefront at the sidewalk edge, maintaining the existing height to width proportion of the street.
- Maintain continuity and rhythm with building materials and architectural details.
- Create attractive street corridors by encouraging store front windows, covered walkways, and highlighted entrances.
- Encourage decorative roof elements and variations in roof height to add visual interest and variety to facades.
- New buildings should maintain established horizontal and vertical lines on the facades of the block.
- The window heights of the new projects in the transit oriented development area should be aligned with other windows heights along the street facade.



New buildings should generally align with the height of adjacent buildings.



Infill building height does not relate to adjacent buildings.



Infill development should maintain sidewalk edge and prevailing building setbacks.

Site Planning

- Structures should be sited in a manner that will compliment adjacent buildings. Sites should be developed in a coordinated manner to provide order and diversity.
- Structures and on-site circulation systems should be located to minimize pedestrian/vehicle conflicts and provide cross-access to adjacent properties.
- Provide convenient sidewalks of adequate width to promote a safe and comfortable alternative to vehicular travel.
- Freestanding singular commercial and service oriented structures should be oriented with their major entry toward the street where access is provided, as well as having their major facade, windows and signage parallel to the street.
- Provide street-facing entries to the individual units whenever possible, to strengthen the commercial/residential streetscape and create a sense of neighborhood among residents.
- Provide human scale along street corridors by encouraging storefront windows, covered walkways, and highlighted entrances.



Building to site relation is poor. Prevailing setbacks are not maintained.



Example of good orientation to major pedestrian walkway and street.



Design Guidelines

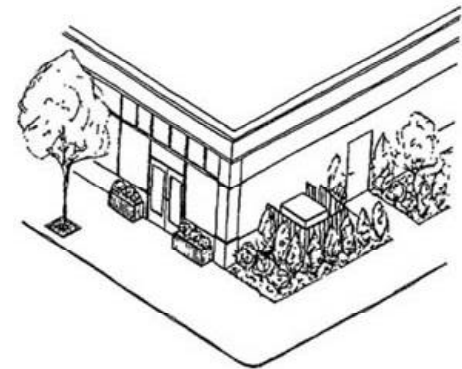
- Avoid separating the development from the neighborhood with high fences, walls, or parking lots.
- Locate dumpsters and mechanical equipment away from the street front, or screen them from view.
- Screen all cooling towers, mechanical equipment or appurtenances, vents, intakes or stacks, or other rooftop structures from view on all sides of the building by a parapet wall, penthouse, or other means. Screens shall be constructed of materials that are compatible with the primary facades.
- Whenever feasible, place all utilities, including overhead service poles underground to minimize their visual impact.



Example of buildings exhibiting good relationships between architectural elements.

Parking and Circulation

- Whenever practical, shared parking between adjacent businesses and/or developments is encouraged to minimize the amount of paved areas.
- Parking access points, whether located on front or side streets must be located as far as possible from street intersections so that adequate stacking room is provided. The number of access points should be limited to the minimum amount necessary to provide adequate circulation.
- Common driveways which provide vehicular access to more than one site are encouraged.
- Where parking areas are connected, direction of travel and parking bays should be similar to reduce conflict at points of connection.
- Parking aisles should be separated from vehicle circulation routes whenever possible.
- First aisle parking stalls should be set back a sufficient distance from the curb to avoid traffic obstruction.
- Drive aisle "throats" should be sufficient depth to avoid vehicle stacking into the street.
- When feasible, separate vehicular and pedestrian circulation systems should be provided. Pedestrian linkages between uses in commercial developments should be emphasized.
- Parking areas should be separated from structures by either a raised concrete walkway or landscaped strip,



Landscape materials and walls/fences should be used for screening service areas and refuse containers.



Parking lot lacks screening and landscaping.



Design Guidelines

preferably both. Situations where parking spaces directly abut structures should be avoided.

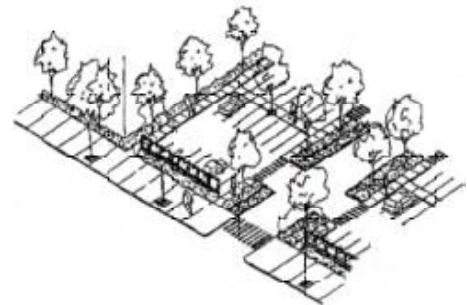
- Break up parking areas with the use of planting islands, walkways, seating areas and medians.
- Parking areas must be landscaped, within the interior as well as perimeter areas of the site.
- Parking areas which accommodate a significant number of vehicles should be divided into a series of connected smaller lots, separated by open space medians, islands, and pedestrian walkways.

Landscaping

- When allowed, exterior storage should be confined to portions of the site least visible to public view. Where screening is required, low landscape hedges or decorative fencing would be appropriate.
- Use adequate buffers and screens, either with landscape, structural, or earthen features to separate vehicular and pedestrian areas.
- Landscaping should define entrances to buildings and parking lots, define the edges of various land uses, provide transition between neighboring properties (buffering), and provide screening for loading and equipment areas.
- Buffers such as fences and vegetation should also be employed to screen parking from roadway areas.
- Locate landscaping to soften the view of parking from dwelling units.
- Balance planting areas with canopy, understory, and groundcover plants as well as with both evergreen and deciduous plant varieties.
- Enhance the character and continuity of the streetscape and walkways with the use of mature vegetation.
- Include special paving and vegetation to highlight building entrances and crosswalks at pedestrian and vehicular intersections.
- Landscaping should be in scale with adjacent structures and be of appropriate size at maturity to accomplish its intended purpose.
- Landscaping should not obstruct visibility at drive aisle intersections.



Parking areas should incorporate distinguished, identifiable pedestrian walkways to create a more pleasant and safer pedestrian experience.



Parking lot plans should include considerable landscaping to screen automobiles from view and to enhance the overall appearance of the lot.



Appropriate example of landscape buffers between parking facilities and sidewalks.



Design Guidelines

- Landscaping should be protected from vehicular and pedestrian encroachment by raised planting surfaces, depressed walks, or the use of curbs.

Signs

- All developments should be designed with a precise concept for adequate signage. Provisions for sign placement, sign scale in relationship with the building, and sign readability should be considered in developing the signing concept.
- All signage should be highly compatible with the building and site design relative to color, material, and placement and should comply with the Village's sign regulations.
- Low monument signs are encouraged as well as integration with landscaping.
- Signs should reflect the architecture and the purpose of the building that it represents.
- Maintain appropriate scale of signs consistent with their use, i.e. larger signs for vehicular traffic areas and smaller signs for pedestrian use areas.
- Limit the number of lettering styles, colors, and pieces of information that are permitted on signs to promote legibility.
- Ensure that way-finding signs are readable from an automobile traveling on the street.
- Separate commercial signs from civic signs.
- Exposed neon tubing is not appropriate material for outdoor signs.

Lighting

- Aim all building or pole mounted lights directly downward only, and floodlights intended to light signage, landscape features, and facades shall be aimed only at those features.
- Minimize the impact of building, street and parking lot lighting on adjacent residential areas.
- No portion of a light source should be exposed below a light fixture.
- Metal halide lighting is recommend; pressurized sodium is discouraged.



Landscaping along the foundation of buildings can soften the edges between the sidewalk and structures.



Signs have poor relation to building architecture and color.



Example of a desirable mounting location for a sign.



Contrast between the sign message and background color increases signs legibility and impact.



Design Guidelines

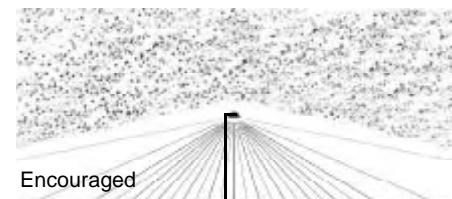
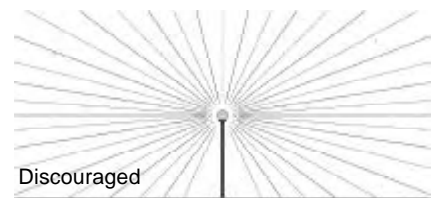
- Use finishes of fixtures that are durable and easily maintained, in neutral colors.
- The design of the light fixtures and their structural support should be architecturally compatible with the main buildings on-site. Illuminators should be integrated within the architectural design for the buildings.
- Use of decorative fixtures for all pedestrian areas to enhance character and lighting is strongly encouraged.
- Lighting should be used to provide illumination for the security and safety of on-site areas such as parking, loading, shipping and receiving, pathways, work areas and intersections.
- All building entrances should be well lit.
- All lighting must comply with the Village's lighting codes and regulations.
- The maximum height of standard lighting shall not exceed 25 feet.

Architecture

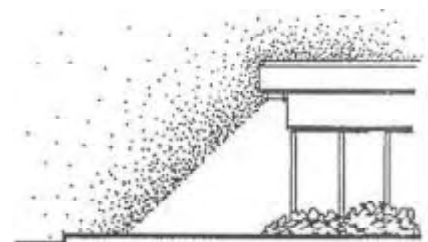
- Scale is the relationship between the size of a new structure, and the size of adjoining permanent structures. Large scale building elements will appear imposing if they are situated in a visual environment which is predominantly smaller in scale.
- The height and scale of new development should be compatible with that of surrounding development. The development should "transition" from the height of adjacent development to the maximum height of the proposed structure.
- The use of brick and stone as primary materials is encouraged. The use of Dryvit and similar materials is acceptable, particularly for building accents.
- The scale, proportions, massing, articulation and design features of the structure should enhance the pedestrian experience.
- The repetition of identical forms creates an undesirable and monotonous streetscape. Therefore, to create variety and interest, the design of structures should be varied and facades should be articulated.



Example of appropriate sign design and color.



Pole mounted lights should be directed downward only.



The impact of pole and building mounted lighting on adjacent properties should be minimized.



Design Guidelines

- Use facade articulation to reduce the perception of bulk and to reflect the traditional rhythm of a residential streetscape.
- Mechanical equipment or other utility hardware should be screened from public view.
- Use roof lines that reflect the surrounding architecture.
- Avoid blank walls facing the street - design street facades with entries, windows, and/or detailing.
- The scale of buildings should be carefully related to adjacent pedestrian areas and other structures.
- Building scale can be reduced through the proper use of window patterns, structural bays, roof overhangs, siding, awnings, moldings, fixtures, and other details.
- Large dominating structures should be broken up by creating horizontal emphasis through the use of trim; adding awnings, eaves, windows, architectural ornamentation; use of complementary colors; and landscape materials.
- The color palette chosen for new structures should be compatible with the colors of adjacent structures and those established in the area.
- Primary colors should be used to accent elements, such as door and window frames and architectural details.
- Hip or gable roof design of appropriate pitch and scale with no exposed utilities or HVAC units is encouraged.
- The use of quality siding or masonry construction of stone or brick on all exterior walls is encouraged.

Commercial

- In mixed use buildings, ground floor spaces are intended for retail and commercial uses only. Upper floors may be commercial, office or residential.
- In-fill development along downtown streets should maintain and reinforce the line of the storefront at the sidewalk edge, maintaining the existing height to width proportion of the street.
- Create attractive and safe pedestrian areas by placing buildings close together and focused towards public areas, and discourage the use of long continuous facades of similar design.



Materials that are similar in texture and character to those established in the development should be used.



The use of brick and stone as primary materials is encouraged.



Buildings with long, monotonous walls should be avoided.

Monotonous walls can be broken up with windows, doors, display cases, architectural detailing, and variation in the height of buildings. A consistent building line should be maintained at the setback along the street.

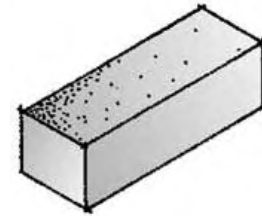


Buildings that avoid long, monotonous walls should be encouraged.

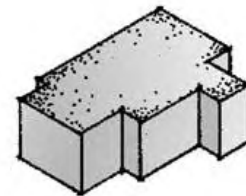


Design Guidelines

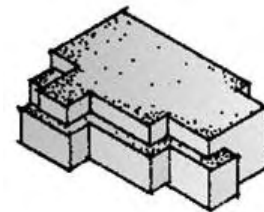
- Pedestrian access from adjacent residential or commercial development is required. Adjacent developments are encouraged to link parking areas and accessways in order to encourage combining of shopping trips and pedestrian activity and reduce redundant driveways.
- Existing building or lot widths should be maintained and should be consistent with adjoining buildings. Proposed buildings that do not conform to this standard should implement designs that reflect the presence of this rhythm of width.
- Encourage the location of off-street parking areas to the side or rear of buildings on each lot. Limited visitor or short term parking may be located in front of the building if necessary, provided such parking is well screened from the right-of-way.
- Maintain continuity and rhythm with building materials and architectural details.
- Encourage decorative roof elements and variations in roof height to add visual interest and variety to facades.
- Create attractive street corridors by encouraging store front windows, covered walkways, and highlighted entrances.
- New buildings should maintain the established horizontal and vertical lines on the facades of the block and the general alignment of building heights should be reinforced.
- Maintain safety and pedestrian comfort within off-street parking and circulation areas.
- Separate incompatible adjacent uses, i.e. parking areas and pedestrian walkways with dense plantings areas.
- Screen trash enclosures, utility boxes, meters, pedestals, and loading/service areas from adjacent properties, public rights-of-way, parking areas and pedestrian walkways. Screening shall consist of a solid wall or fence, or landscape material, equal in height or taller than the material being screened.
- Use landscape treatment around the base of retail buildings to enhance the appearance of structures and soften views of paved parking areas.



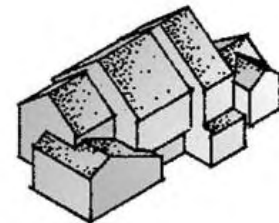
Undesirable architectural treatment.



Vertical articulation added.



Horizontal articulation added.



Multi-planned roofs and awnings add desirable articulation.



Commercial storefront entries should be oriented towards the principle street frontage. When appropriate, pedestrian amenities should be provided.



Design Guidelines

Residential

- Wherever possible, front yard setbacks should be minimized to enhance the relationship between the individual units or structures and the street. Front yard setbacks of any new construction should continue the prevailing setback patterns of adjacent buildings.
- The scale and proportion of the new construction must be compatible with adjacent buildings to create unity and harmony of the streetscape.
- Materials used for new construction should be compatible with those used in neighboring buildings, and should not stand out in contrast to the rest.
- The mix of materials on the facades of structures and garage doors should be consistently applied and should be chosen to work harmoniously with adjacent materials.
- The scale of the structures should be within a human scale, reflective of traditional neighborhoods, so as not to overwhelm or dominant its surroundings.
- Integrate the site plan of new residential development with the surrounding neighborhood.
- Sidewalks, planting strips and street trees should be incorporated into the landscape plan for all new residential developments. Safe pedestrian pathways from parking areas to unit entrances should be provided.
- Roof lines should be representative of the design and scale of the structure and the surrounding dwellings.
- For sloped roofs, both vertical and horizontal articulation is encouraged.
- In multifamily structures facades should be broken up to give the appearance of a collection of smaller structures. This articulation can be accomplished with the use of balconies, setbacks and projections. To the extent possible, each unit of a multifamily structure should be individually identifiable.
- Townhome “clusters” should include no more than six dwelling units per group.
- The preferable arrangement of townhomes is oriented around a joint-use open space area, or a “commons”. The commons should be oriented toward the primary street.



The incorporation of offsets and jogs in the roofline the vertical articulation adds interest and should be encouraged.



Building which incorporates similar architectural styles and design features, building materials, and colors complementary to those used in surrounding buildings.

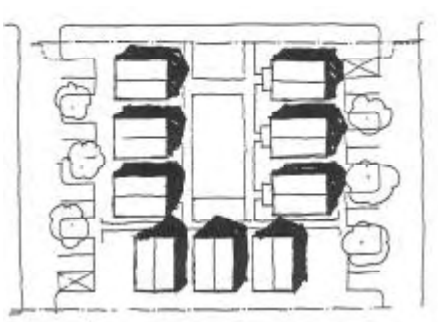


Example of building incorporating shutters, lintels and a strong cornice line. The use of vertical elements to minimize mass should also be noted.



Design Guidelines

- For multifamily structures, long, unbroken facades and box-like forms should be avoided. Treat each street facade with the same level of formality afforded to single family home design.
- The visual impact of garages should be concealed or minimized.
- Garage doors should relate to the particular architectural style selected for the structure it serves.
- Garages should never dominate the facade of any residential structure.
- Orientation of the garage door away from the street is preferred. Whenever possible, garages should be located at the rear of a lot with access provided by alleyways.
- If garages must be oriented to the street, varied garage placements are encouraged to avoid a monotonous streetscape.
- In higher density projects, large monotonous parking lots and undivided parking lots should be avoided.
- Parking lots should be substantially landscaped to minimize their visual impact.



The preferable arrangement of townhomes is oriented to common open space.



Example of a mixed use building. Residential units are not allowed on the first floor.



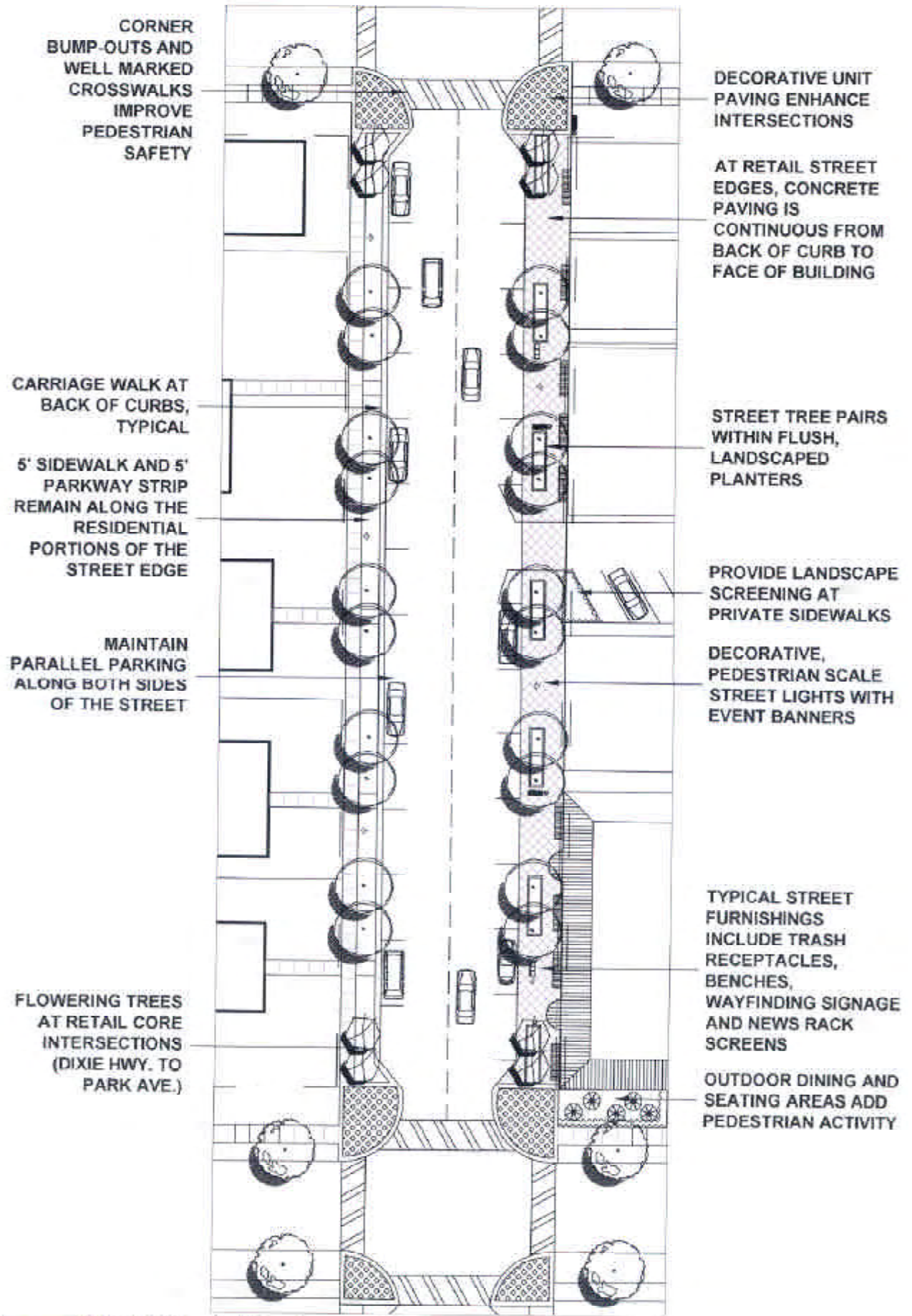
All multiple family facades should be treated with the same level of formality as a single family home.



Sloped roofs with vertical and horizontal articulation are encouraged.



Balconies, setbacks, and projections can be used to give the appearance of a collection of smaller structures.



View of shopping center along Dixie Avenue



Existing



Proposed

Implementation

The Hazel Crest Proper Revitalization Action Plan contains a specific outline of tasks needed to redevelop the study area. Developed in late 2000, this document outlines tasks, responsibility, and costs. As the Village's Economic Development Task Force will be the group responsible for spearheading redevelopment efforts, it is recommended that this group review and adjust this Action Plan as an early step in the redevelopment process. For example, the group will need to prioritize efforts and make adjustments to the timeline given current conditions and availability of staff to spearhead the effort.

Some of the cost estimates contained in the original Action Plan were very general since the design work contained in this document had not yet been completed. To provide a better estimate of potential community or public sector improvements, a detailed cost estimate is included in the appendix. Possible funding for these improvements is discussed below, but is primarily anticipated to come from TIF sources. These cost estimates include streetscape enhancement along 170th Street, Dixie Highway, and Wood Street. Also included are estimates for community wayfinding signs, Page Street abandonment, and Bicentennial Park reconfiguration and redesign. Parking, streetscape and embankment improvements along Park Avenue are estimated, as are enhancements to the station entrance. Estimates are also provided for improvement to the existing Village commuter lot at 170th Street and Park Avenue and the proposed lot at site 10B.

Zoning

Several zoning map amendments are suggested in the redevelopment strategy table, page 3-1. It is recommended that these zoning amendments occur as private development projects are proposed on individual parcels. As an incentive to promote redevelopment, the Village may choose to wave application fees if the proposed rezoning is consistent with recommendations outlined in the redevelopment table.

To permit the creative layout of townhome developments as suggested by the concept plan, it is recommended that redevelopment of sites 5 and 20 be handled as R-3 Planned Developments. This will allow greater review of development layout, and permit greater flexibility in the design of these multi-family areas. In particular, deviation from standard front, rear, and side yard setbacks may be needed to accommodate the courtyard design suggested on the concept plan.



Implementation

Tax Increment Financing

The Village of Hazel Crest established a Tax Increment Financing District (TIF) for much of the commercial areas within the Hazel Crest Proper area in January of 2001. Projects within this district are eligible for redevelopment assistance for a variety of categories consistent with state statutes, including but not limited to property assembly, infrastructure improvements, and financing and relocation costs. Requests for TIF assistance should outline projected cash flows, and should demonstrate that sufficient tax increment will be available after project completion to cover the cost of any bond issuance. In the TIF Redevelopment Plan, total estimated project costs are listed as \$12,000,000. This calculation was based on a net present value analysis of incremental property tax revenue. This was a conservative estimate, and is still a valid estimate for planning purposes. However, based on the specific development and density outlined on the Concept Plan, incremental revenues could be significantly higher. The end equalized assessed value (EAV) of property in the TIF District was estimated at approximately \$18,000,000 if all projects outlined in the original redevelopment plan are completed. With the additional projects suggested by this Concept Master Plan, end EAV could be as high as \$29,000,000.

Other Tax Incentives

Tax Savings for Commercial with Apartments Above

Cook County has a relatively new tax incentive program known as “Small Business with Apartments Above.” This new property classification by the Cook County Assessor provides a substantial property tax savings for commercial development that has retail or office on the ground floor and rental apartments above. The classification applies to retail or office space with rental apartments above, with a maximum of six spaces (commercial and apartments together) and 20,000 square feet of above-ground floor space. Properties meeting this classification are assessed as residential (16%) rather than commercial (38%).

This program may be appropriate for existing vacant redevelopment sites along 170th Street. On these vacant sites, the tax increment, even at the lower assessment rate, should not have a significantly negative effect on the TIF District. The higher property value resulting from new construction should offset the reduced assessment rate. On property that is already improved, use of this incentive would likely suggest that TIF funding should not be utilized on the same parcel. In the case of already improved property that is now assessed at the commercial rate, a reduction in the assessment would, even with new construction, make it difficult to create a positive tax increment to fund improvements.



Implementation

Sales Tax Rebate

To encourage retail development, some municipalities will pledge a portion of their sales tax revenue from a particular retail store as an incentive to develop. This typically works well for large developments, where significant sales tax revenue is anticipated. However, within this study area most users will be relatively small and will likely not produce significant sales tax revenue. Therefore, the use of a sales tax rebate should only be considered for the large commercial center planned along Wood Street.

Other Funding Sources

The TIF Redevelopment Plan outlines a number of potential non-tax related funding sources for redevelopment, including:

Gifts, grants, and contributions

Community Development Block Grant (CDBG) funding has been used in the past in this neighborhood and may be able to provide some minor funding for recommended improvements, particularly capital projects. Improvements such as the redesign of Bicentennial Park are good projects to seek gifts or contributions to fund improvements. The Hazel Crest Park District and the Village will need to work closely on park development and funding to revitalize this strategically located park.

Sale or lease of land proceeds

The Village already controls several small parcels within the study area. In particular, Site 16 is currently owned by the Village and is outside of the TIF District. The sale of this parcel at a discounted price could be used as an incentive to promote redevelopment of this site. The remaining proceeds from the sale could then be funneled back into other projects recommended on the Concept Master Plan such as wayfinding signs.

The Village's general revenue fund

All of the public capital improvements suggested in the Concept Master Plan could be funded through the Village's general revenue sources if funds were available. However, given existing municipal finances it is not anticipated that significant general revenue funds will be available for major projects within the study area.



Appendix

Hazel Crest TOD Survey

- Summary of Hazel Crest Station Users and Hazel Crest Residents Responses.
- Hazel Crest Residents' most desired addition to the station area was a coffee stand (47.9%) while Hazel Crest station users top choice was an ATM (44.4%).
- High percentages of both groups want a convenience store, 45.9% of residents and 43.7% of station users.
- Fast Food, dry cleaners and a newsstand were also selected by more than a third of each group.
- 99% of station users and 97% of residents reported that they would patronize station area businesses.
- Only 16% of station users and 13.7% of residents would walk more than two blocks out of their way to patronize a station area business.
- 82.6% of station users and 83.5% of residents felt that having businesses near Metra makes using Metra more desirable.
- 68.5% of station users and 72.8% of residents reported that the Metra station was important in their decision where to live.
- 91.5% of station users and 93.1% reported that having Metra near a business makes the business more desirable.
- 73.4% of station users and 81.2% of residents shop at stores that they would not know of except for seeing them from the train.
- Over 90% of all respondents felt that their cars were safe in their parking space and that they felt safe walking from their parking space to the station.
- Over 90% of station users and residents were in favor of adding store space, restaurant space, office space and condominiums for buyers of all ages to the Hazel Crest



Appendix

station area. The support for medical/dental offices, townhomes and senior condominiums was only slightly lower with over 80% of both groups in favor of those developments.

- 82.6% of station users and 84.9% of residents favored selling the station area park to benefit other community park facilities.
- 83.5% of station users and 87% of residents supported combining the resources of public agencies to build a community center in the station area.

Survey Results

Station Development

Please specify the businesses that you would be most likely to patronize in a station area.

	Hazel Crest Station	Hazel Crest Residents
N	135	146
Fast Food Restaurant	41.50%	40.40%
Auto Repair Shop	7.40%	7.50%
Convenience Store	43.70%	45.90%
Cards/Gifts Shop	20.00%	19.90%
ATM	44.40%	35.65%
Daycare Center	18.50%	15.80%
Drugstore	22.20%	19.90%
Newsstand	39.30%	35.60%
Hardware Store	2.20%	4.10%
Dry cleaner	37.00%	34.90%
Family Style Restaurant	15.60%	13.00%
Bank	13.30%	11.00%
Coffee Stand	40.00%	47.90%
Video Rental	20.00%	21.20%
Grocery Store	23.70%	25.30%
Gas Station	11.90%	13.70%



Appendix

When would you patronize these businesses?

	Hazel Crest Station	Hazel Crest Residents
N	125	140
AM	8.00%	8.60%
PM	20.80%	16.40%
Both	70.40%	72.10%
Neither	0.80%	2.90%

On your way to or from the train, what is the furthest distance you would walk out of your way to patronize a business?

	Hazel Crest Station	Hazel Crest Residents
N	125	138
One Block	43.20%	41.30%
Two Blocks	19.20%	23.90%
Three Blocks	4.00%	3.60%
Four or More Blocks	12.00%	10.10%
I would not walk out of my way	21.60%	21.10%

Please give us your opinion by checking the box below your reaction to the following statements.

Hazel Crest Station

	N	Strongly agree	Agree	Disagree	Strongly disagree	No opinion
1. Having businesses near the Metra Station makes using Metra more desirable	115	8.70%	73.90%	13.90%	3.50%	20
2. Having Metra near the businesses makes the businesses more desirable.	117	17.10%	74.40%	7.70%	0.90%	18
3. The Metra station was an important factor in my decision where to live.	111	10.80%	57.70%	27.00%	4.50%	24
4. I shop at stores that I would not know of except for seeing them on my way to or from the train.	109	4.60%	68.80%	24.80%	1.80%	26
5. I would be more likely to use businesses if they were located near the station building.	113	13.30%	69.00%	16.80%	0.90%	22
6. I would rather shop at businesses near my home than at similar businesses near the Metra station.	112	7.10%	49.10%	39.30%	4.50%	23
7. I believe that my car is secure parked in a station area lot.	75	8.00%	82.70%	5.30%	4.00%	60
8. I feel safe walking from my parking space to the Metra station.	77	5.20%	85.70%	6.50%	2.60%	58



Appendix

Hazel Crest Residents

	<i>N</i>	<i>Strongly agree</i>	<i>Agree</i>	<i>Disagree</i>	<i>Strongly disagree</i>	<i>No opinion</i>
1. Having businesses near the Metra Station makes using Metra more desirable	127	8.70%	74.80%	14.20%	2.40%	19
2. Having Metra near the businesses makes the businesses more desirable.	130	16.20%	76.90%	6.90%	0.00%	16
3. The Metra station was an important factor in my decision where to live.	121	11.60%	61.20%	24.00%	3.30%	25
4. I shop at stores that I would not know of except for seeing them on my way to or from the train.	117	6.80%	74.40%	17.10%	1.70%	29
5. I would be more likely to use businesses if they were located near the station building.	119	14.30%	75.60%	10.10%	0.00%	27
6. I would rather shop at businesses near my home than at similar businesses near the Metra station.	127	6.30%	50.40%	40.20%	3.10%	19
7. I believe that my car is secure parked in a station area lot.	91	9.90%	82.70%	2.20%	3.30%	56
8. I feel safe walking from my parking space to the Metra station.	91	7.70%	85.70%	3.30%	1.10%	56

The Village of Hazel Crest is planning to improve the Hazel Crest station area by encouraging renovation of existing buildings and the addition of new buildings. Please give us your opinion of potential changes by checking the box next to your reaction to the following statements.

Hazel Crest Station

	<i>N</i>	<i>Strongly agree</i>	<i>Agree</i>	<i>Disagree</i>	<i>Strongly disagree</i>	<i>No opinion</i>
1. Private Developers should be encouraged to add:						
Store space	105	14.30%	81.90%	3.80%	0.00%	30
Restaurant space	107	14.00%	79.40%	5.60%	0.90%	28
Office space that provides employment opportunities.	105	16.20%	78.10%	5.70%	0.00%	30
Medical and or dental office space	98	14.30%	72.40%	12.20%	1.00%	37
Senior condominium housing	94	12.80%	75.50%	9.60%	2.10%	41
Condominium housing open to residents of all ages	91	15.40%	74.70%	6.60%	3.30%	44
Townhouses	89	9.00%	77.50%	12.40%	1.10%	46
2. The Hazel Crest Park District should be encouraged to sell the station area park to raise funds to improve other parks and to provide new redevelopment opportunities.	98	21.40%	61.20%	11.20%	6.10%	37
3. Area public agencies should combine resources to build a community center near the Hazel Crest Metra Station.	91	9.90%	73.60%	14.30%	2.20%	44



Appendix

Hazel Crest Residents

	<i>N</i>	<i>Strongly agree</i>	<i>Agree</i>	<i>Disagree</i>	<i>Strongly disagree</i>	<i>No opinion</i>
1. Private Developers should be encouraged to add:						
Store space	120	18.30%	76.70%	5.00%	0.00%	26
Restaurant space	114	16.70%	78.90%	4.40%	0.90%	32
Office space that provides employment opportunities.	114	20.20%	71.10%	7.00%	0.00%	32
Medical and or dental office space	109	13.80%	77.10%	8.30%	1.00%	37
Senior condominium housing	111	16.20%	67.60%	16.20%	2.10%	35
Condominium housing open to residents of all ages	106	20.80%	69.80%	8.50%	3.30%	40
Townhouses	103	14.60%	68.00%	16.50%	1.10%	43
2. The Hazel Crest Park District should be encouraged to sell the station area park to raise funds to improve other parks and to provide new redevelopment opportunities.	106	21.70%	63.20%	9.40%	6.10%	40
3. Area public agencies should combine resources to build a community center near the Hazel Crest Metra Station.	100	16.00%	71.00%	11.00%	2.00%	46

Transit Behavior & Demographics

Please note location of the place you began your trip to the Metra station this morning.

	Hazel Crest Station	Hazel Crest Residents
<i>N</i>	135	146
Harvey	0	11.00%
Hazel Crest	100%	45.20%
Calumet	0	43.80%

Is this station where you boarded the closest train station to where you began your trip?

	Hazel Crest Station	Hazel Crest Residents
<i>N</i>	127	139
Yes	94.50%	94.20%
No	5.50%	5.80%



Appendix

How did you get to the train station this morning?

	Hazel Crest Station	Hazel Crest Residents
N	130	138
Walked	12.30%	5.80%
Drove alone and parked	29.20%	33.30%
Carpool driver	6.90%	8.70%
Carpool passenger	13.10%	12.30%
Got dropped off	38.50%	39.90%
Bicycled	0.00%	0.00%
Took bus	0.00%	0.00%
Other	0.00%	0.00%

If you parked at the station, please refer to the maps on the back page. Write the number that corresponds to the parking lot that you used today.

	Hazel Crest Station	Hazel Crest Residents
N	38	54
1	23.70%	3.70%
2	21.10%	9.30%
3	15.80%	13.00%
4	2.60%	5.60%
5	2.60%	5.60%
6	2.60%	11.10%
7	0	9.30%
8	2.60%	22.20%
9	0.00%	7.40%
10	2.60%	5.60%
11	0.00%	0.00%
12	2.60%	7.40%

What type of Metra ticket are you using?

	Hazel Crest Station	Hazel Crest Residents
N	132	139
Monthly	55.30%	53.20%
One-way	8.30%	12.20%
10-ride	36.40%	34.50%
Other	0.00%	0.00%



Appendix

What is the primary purpose for your trip today?

	Hazel Crest Station	Hazel Crest Residents
N	120	130
Work	90.00%	96.20%
School	8.30%	3.80%
Other	1.70%	0.00%

About how long have you used Metra services?

	Hazel Crest Station	Hazel Crest Residents
N	126	143
Over three years	35.70%	33.60%
Between 2-3 years	15.10%	11.20%
Between 1-2 years	24.60%	22.40%
6 months to one year	13.50%	17.50%
Less than one year	8.70%	11.90%
Not a regular user	2.40%	3.50%

Including yourself, how many persons live in your household?

	Hazel Crest Station	Hazel Crest Residents
N	116	138
1	15.50%	15.20%
2	15.50%	14.50%
3	23.30%	21.00%
4	21.60%	26.80%
5	16.40%	15.20%
More than 5	7.70%	7.30%

How many persons in your household are employed?

	Hazel Crest Station	Hazel Crest Residents
N	111	131
1	35.10%	44.30%
2	56.80%	47.30%
3	7.20%	6.10%
More than 3	0.90%	2.30%



Appendix

What was your household's total 2000 income before taxes?

	Hazel Crest Station	Hazel Crest Residents
N		
Average	\$65.50	\$66.90

Sex

	Hazel Crest Station	Hazel Crest Residents
N	126	138
Male	30.20%	30.40%
Female	69.80%	69.60%

What is your age?

	Hazel Crest Station	Hazel Crest Residents
N	118	132
Average	38.1	40.3

Methodology

On September 19, 2001 from 6:30 A.M. to 9:00 A.M., a total of 1,500 surveys were distributed to passengers entering these Metra Electric Line Stations: Harvey, Hazel Crest and Calumet. Those three stations were chosen to capture Hazel Crest residents who board at a different station due to the limited parking at the Hazel Crest station. The passengers were allowed to complete the surveys while awaiting their trains or to return them to survey personnel who greeted evening trains. Drop boxes were also available at the Hazel Crest Municipal Center. Survey personnel at stations during the return commute collected all but four responses that were returned at the Hazel Crest Municipal Center. The return rate was 27.5% with this station distribution:

N	413
Harvey	27.80%
Hazel Crest	32.70%
Calumet	39.50%
Other	0.00%

It is important to note that, during the morning distribution, it was drizzling which probably made passengers slightly less willing to accept and complete the survey. Also, this survey proceeded as scheduled only eight days after September 11, 2001, which may have had some impact on responses.



Appendix

A comparison with projections for the total Hazel Crest Population by a national demographics service suggests that the results are consistent with expectations for a balanced sample of commuters using these stations.

<i>Category</i>	<i>Survey</i>	<i>Experion Database</i>
% Female	69.60%	52.40%
Average Household Income	\$66,900	\$63,752
Average Age	40.3	33
Average Household Size	3.4	2.99

In general, females are more likely to complete surveys and previous Metra surveys of this station revealed that there is a higher than average number of female commuters at these stations. Therefore, the high percentage of female respondents seems appropriate. Because children do not complete surveys, it is expected that the average age of survey respondents would be higher than the population as a whole. Because the older residents who typically live in one or two person households are likely to be retired and therefore not commuting the larger average household size also is reasonable.



Design Guidelines Definitions

Aesthetic: Pertaining to art, taste, or beauty.

Appearance: The outward aspect visible to the public.

Appropriate: Sympathetic, or fitting, to the context of the site and the whole community.

Appurtenances: The visible, functional object accessory to and part of buildings.

Architectural Character: The composite or aggregate of the characteristics of the structure, form, materials, and function of a building, group of buildings, or other architectural composition.

Architectural Concept: The basic aesthetic idea of a building, or group of buildings or structures, including the site and landscape development, which produces the architectural character.

Architectural Feature: A prominent or significant part or element of a building, structure, or site.

Architectural Style: The characteristic form and detail, as of buildings of a particular historic period.

Attractive: Having qualities that arouse interest and pleasure in the observer.

Awning: A temporary and movable covering extending from a building to over the sidewalk or entrance; usually made of a fabric-type material.

Bay: That compositional and structural subdivision of building which, through repetition, makes up the total design of a building.

Baluster: One of a row of short pillars or columns in a balustrade.

Balustrade: A handrail or a coping, together with the short pillars of columns (balusters) which support it.

Berm: A raised form of earth to provide screening or improve the aesthetic character of the grade.

Buffer: A landscaped area or strip intended to separate and partially obstruct the view of two adjacent land uses or properties from one another.

Bulkhead: The structural panels just below display windows on storefronts. Bulkheads can be both supportive and decorative in design. Bulkheads from the 19th century are often of wood construction with rectangular raised panels while those of the 20th



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century may be of wood, brick tile, or marble construction. Bulkheads are often referred to as kickplates.

Canopy: Permanent awning made out of a solid material; used in the building design.

Casement: 1. A window consisting of one or more sashes which swing open on hinges at their vertical edges. 2. A sash used in such a window.

Clapboard: Thin long boards, with the bottom edges thicker than the top edges, overlapped horizontally, and used over timber-framed structures to provide a weatherproof exterior wall.

Coherent: Easily appreciated visually by reason of consistency and compatibility; i.e., everything obviously belonging together.

Cohesiveness: Unity of composition between design elements of a building or a group of buildings and the landscape development.

Compatibility: Harmony in the appearance of two or more external design features or elements in the same vicinity.

Conservation: The protection and care which prevent destruction or deterioration of historical or otherwise significant structures, buildings, or natural resources.

Context: Aesthetic character or vicinity of an area under review; Surroundings, background for some object.

Coping: 1. A protective capping or covering at the top of a masonry wall. 2. A horizontal member which together with the balusters supporting it constitutes a balustrade.

Corbel: A bracket of brick or stone built out from a wall.

Cornice: Any projecting ornamental molding along the top of a building.

Cupola: 1. A dome. 2. A small construction, not necessarily a dome, rising above the roof of a building, which may be purely decorative, but is often used as a means of admitting light and air.

Decorative Screen: A wall or fence intended to partially or entirely cut off visibility to the area behind it.

Dentil Course: A horizontal row of small projecting rectangular blocks supporting a superimposed molding.

Design Concepts: Refers to architectural and site planning and to the analysis of the whole structure in terms of form, proportion, color, material, and textures.



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Element: A device used to create an image.

Exterior Building Component: An essential and visible part of the exterior of a building.

External Design Feature: The architectural style and general arrangement of such portion of a building or structure as is to be open to view from a public street, place, or way, including the kind, color, and texture of the building material of such portion and the type of windows, doors, and lights attached, or ground signs and other fixtures appurtenant to such portion.

Facade: The elevation or exterior face of a building.

Fenestration: The design and disposition of windows and other exterior openings of a building

Eaves: The lower edge of a sloping roof, projecting beyond the wall.

Fillet: 1. A narrow ribbon-like flat band or molding. 2. The flat surface between the flutes of an Ionic column.

Finial: An ornament at the top of a spire, pinnacle, gable, post, or other architectural element, to serve as its terminal feature.

Gable: The portion of the vertical end wall of a building above the eaves line to the ridge, generally but not necessarily triangular in shape.

Graphic Element: A letter, illustration, symbol, figure, insignia or other device employed to express and illustrate a message or part thereof.

Harmony: A quality which represents an appropriate and congruent arrangement of parts, as in arrangement of varied architectural and landscape elements.

Landscape: Plant materials, topography, and other natural physical elements combined in relation to one another and to man-made structures.

Light Cut-Off Angle: An angle from vertical extending downward from a luminary which defines the maximum range of incident illumination outward at the ground plane.

Line: Refers to the vertical and horizontal alignment of architectural features that occur on individual building facades and on adjacent building fronts.

Lintel: A horizontal beam spanning an opening.

Logic of Design: Accepted principles and criteria of validity in the solution of the problem of design.



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Massing: Refers to the relationship of visual volume of one building to its neighbors.

Mechanical Equipment: Equipment, devices and accessories, the use of which relates to water supply, drainage, heating, ventilating, air conditioning and similar purposes.

Miscellaneous Structures: Structures, other than buildings, visible from the public ways. Examples are: memorials, staging, antennas, water tanks and towers, sheds, shelters, fences and walls, kennels, transformers and drive-up facilities.

Module: That unit of a design by which all other elements of the design are proportioned. In classical architecture, the half diameter of a column was used as a module.

Mullion: A vertical member subdividing a window opening into separate lights.

Parapet: A solid wall of handrail height placed at the edge of a roof, balcony, or other similar horizontal surface.

Pinnacle: An ornament having the basic form of an obelisk or a slender pyramid and used as a terminal feature to cap a buttress or to accent corners or high points of a roof or tower.

Plant Materials: Trees, shrubs, vines, ground covers, grass, perennials, annuals, and bulbs.

Portico: 1. Generally, a colonnade or arcade serving as the entrance to a building. 2. A colonnaded or arcaded walkway, either freestanding or attached to a building.

Proportion: Balanced relationship of parts of a building, landscape, structures or buildings to each other and to the whole.

Renovation: The interior or exterior remodeling of a structure, other than ordinary repair.

Rhythm: The pattern of occurrence of related elements.

Sash: 1. A frame into which glass is set. 2. Especially each of the two vertically sliding frames of a sash window.

Sash Window: 1. A window consisting of one or more sashes. 2. Especially, a window made up of a pair of sashes, which opens by sliding one or both sashes vertically, to distinguish from a casement.

Scale: Proportional relationship of the size of parts to one another and to the human figure.



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Scallop: An ornament in the form of a scallop shell.

Scalloping: A series of ornamental adjoining semicircular forms along an edge.

Screening: Structure or planting which conceals from view from public ways the area behind such structure or planting.

Shrub: A multi-stemmed woody plant other than a tree.

Site Break: A structural or landscape device to interrupt long vistas and create visual interest in a site development.

Soffit: The exposed underside of an architectural member.

Spire: A slender conical or tapered polygonal structure placed on top of a roof or tower.

Street Hardware: Man-made objects other than buildings which are part of the streetscape. Examples are: lamp posts, utility poles, traffic lights, traffic signs, benches, litter containers, letter boxes, fire hydrants.

Streetscape: The scene as may be observed along a public street or way composed of natural and man-made components, including buildings, paving, planting, street hardware and miscellaneous structures.

Street Wall: Any wall fronting a street. A street wall line extends outward, from the outermost points of each building's street wall, parallel to the street, until such extensions of said line intersect the side and/or rear property line, encircle the building, or intersect another wall line. (If a building has a rounded front or the building is on an irregular shaped lot, the points of the street wall closest to the side property lines shall be used to determine the street wall line.)

String Course: A projecting horizontal band or ornamental molding built into the exterior face of a masonry wall, often indicating the floor level.

Structure: Anything constructed or erected, the use of which requires permanent or temporary location on or in the ground.

Symmetry: Having parts similar in shape and measurement identically arranged on opposite sides of a center line (axis).

Transom: A horizontal opening (or bar) over a door or window

Utilitarian Structure: A structure or enclosure relating to mechanical or electrical services to a building or development.



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Utility Hardware: Devices such as poles, crossarms, transformers and vaults, gas pressure regulating assemblies, hydrants and buffalo boxes that are used for water, gas, oil, sewer and electrical services to a building or a project.

Utility Service: Any device, including wire, pipe, and conduit which carries gas, water, electricity, oil, and communications into a building or development.





Figure 14: Parking Assessment Location



Appendix

Parking Analysis - Off Street Parking

Block	Lot Designation	Space Inventory	A.M. Occupancy	Percent Occupied	P.M. Occupancy	Percent Occupied
1	School Admin. Staff	5	5	100%	5	100%
2	Unmarked Lot	3	0	0%	0	0%
	Marked Lot	6	0	0%	0	0%
	Marked Angle Parking	5	0	0%	0	0%
	Subtotal	14	0	0%	0	0%
3	170th/Jodave	25	3	12%	5	20%
	170th/Lincoln	15	7	47%	7	47%
	Subtotal	40	10	25%	12	30%
4	Behind Building	25	11	44%	10	40%
5	170th/Wood	10	3	30%	3	30%
	Apartment Complex	13	7	54%	7	54%
	Subtotal	23	10	43%	10	43%
6	Metra Lot	94	85	90%	81	86%
7	None	0	0	0%	0	0%
8	Beef Hut @ Wood Street	7	2	29%	2	29%
	N/O Alley Apartment - Unmarked Lot	7	4	57%	4	57%
	Apartment - Marked Lot	15	10	67%	10	67%
	Unmarked Lot	4	2	50%	2	50%
	S/O Alley Marked Lot	12	12	100%	12	100%
Subtotal	45	30	67%	30	67%	
9	Metra Lot	50	50	100%	49	98%
	Beauty Salon	6	6	100%	6	100%
	Subtotal	56	56	100%	55	98%
10	Behind Building	5	2	40%	2	40%
	Tenant Parking	5	3	60%	3	60%
	Subtotal	10	5	50%	5	50%
11	None	0	0	0%	0	0%
12	Dental Office	3	0	0%	0	0%
	Apartment Building	7	0	0%	0	0%
	170th/Roby	4	2	50%	2	50%
	Subtotal	14	2	14%	2	14%
Total		326	214	66%	210	64%

